

ATTACHMENT TO AGENDA ITEM

Ordinary Meeting

20 March 2018

Agenda Item 10.4	Adoption of Amendment C196 to the Greater Shepparton Planning Scheme (Addendum to the Urban Design Framework - Shepparton North and South Business Areas, July 2017)	
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G R E A T E R S H E P P A R T O N
G R E A T E R F U T U R E



MINUTES

FOR THE
GREATER SHEPPARTON CITY COUNCIL

ORDINARY COUNCIL MEETING

HELD ON
TUESDAY 18 JULY, 2017
AT 5.30PM

IN THE COUNCIL BOARDROOM

COUNCILLORS:

Cr Dinny Adem (Mayor)
Cr Kim O'Keeffe (Deputy Mayor)
Cr Seema Abdullah
Cr Bruce Giovanetti
Cr Chris Hazelman
Cr Les Oroszvary
Cr Dennis Patterson
Cr Fern Summer
Cr Shelley Sutton

VISION

*A THRIVING ECONOMY IN THE FOODBOWL OF VICTORIA WITH
EXCELLENT LIFESTYLES, INNOVATIVE AGRICULTURE
A DIVERSE COMMUNITY AND
ABUNDANT OPPORTUNITIES*



10. SUSTAINABLE DEVELOPMENT DIRECTORATE

10.3 Adoption of Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017

Disclosures of conflicts of interest in relation to advice provided in this report
Under section 80C of the *Local Government Act 1989* officers and persons engaged under a contract providing advice to Council must disclose any conflicts of interests, including the type and nature of interest.

No Council officers or contractors who have provided advice in relation to this report have declared a conflict of interest regarding the matter under consideration.

Council Officers involved in producing this report

Author: Senior Strategic Planner

Proof reader(s): Team Leader Strategic Planning

Approved by: Director Sustainable Development

Executive Summary

At the Ordinary Council Meeting held on 20 June 2017, Council resolved to adopt the *Urban Design Framework: Shepparton North and South Business Areas (Addendum) 2017* (the UDF Addendum) and note Council officers' intention to prepare and exhibit a planning scheme amendment to implement its recommendations into the Greater Shepparton Planning Scheme, including the Design and Development Overlays (see *Attachment 1 – Unconfirmed Minutes Extract from June 2017 Ordinary Council Meeting*).

The recently updated Ministerial Direction *Form and Content of Planning Schemes* included changes to Design and Development Overlays (DDOs). The Department of Environment, Land, Water and Planning (DELWP) is aiming to reduce the size and complexity of such provisions, including limiting the amount of objectives you can have in a DDO. The new Ministerial Direction has also placed greater emphasis on ensuring consistency between strategy plans and planning schemes, i.e. that the provisions in the planning scheme directly relate back to the source of those provisions, in this instance being the UDF Addendum.

Since the 20 June 2017 Council resolution, we have started the process of seeking authorisation from the Minister for Planning for the planning scheme amendment to implement the UDF Addendum and the new/ revised DDOs.

Given that Council received submissions in relation to the content/recommendations of the UDF Addendum when it was on exhibition, and is now looking to seek authorisation from the Minister for Planning, DELWP has since advised that it is necessary to update the UDF Addendum to reflect these changes and make sure its recommendations align with any planning scheme documentation we are looking to introduce.

Additionally, some of the submitters have advised that they want further clarification around the gateway sites than what the document (adopted in June) provides for. It provides greater clarity and direction to also make these minor changes before exhibition.

The UDF Addendum has now been revised to align with this new Ministerial Direction. It is recommended that Council adopt the revised *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017* (see *Attachment 2 - Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017*) and prepare and exhibit a planning scheme amendment to implement its recommendations.



10. SUSTAINABLE DEVELOPMENT DIRECTORATE

10.3 Adoption of Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017 (continued)

Moved by Cr Hazelman
Seconded by Cr Patterson

That the Council:

1. adopt the revised *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017*;
2. note Council officers' intention to prepare and exhibit a planning scheme amendment to include the recommendations of the *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017* into the Greater Shepparton Planning Scheme, including the Design and Development Overlays; and
3. note that future budget bids will be made to implement the findings and recommendations of the *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017* to plan and improve the public realm in Shepparton North.

CARRIED.

Background

The *Urban Design Framework: Shepparton North and South Business Areas* was originally prepared by Coomes Consulting Pty Ltd in 2006 (the 2006 UDF). In December 2016, Planisphere Pty Ltd was engaged to review the 2006 UDF.

At the Ordinary Council Meeting held on 20 June 2017, Council resolved to adopt the *Urban Design Framework: Shepparton North and South Business Areas (Addendum) 2017* (the UDF Addendum) and note Council officers' intention to prepare and exhibit a planning scheme amendment to implement its recommendations into the Greater Shepparton Planning Scheme, including the Design and Development Overlays (see *Attachment 1 – Unconfirmed Minutes Extract from June 2017 Ordinary Council Meeting*).

The UDF Addendum provides new urban design frameworks for two key areas, being Shepparton North and the entrance to Kialla Lakes Drive, and includes recommendations regarding their implementation into the Greater Shepparton Planning Scheme.

The UDF Addendum recommends that a new Schedule to the Design and Development Overlay be applied to the Shepparton North study area. A draft Schedule to the Design and Development Overlay is included in the UDF Addendum.

Regarding the Kialla Lakes Drive study area, the UDF Addendum recommends that the existing Design and Development Overlay – Schedule 7 (DDO7) is extended to include the study area and revised as relevant. A draft revised Schedule to the Design and Development Overlay is included in the UDF Addendum.

The recently updated Ministerial Direction *Form and Content of Planning Schemes* included changes to Design and Development Overlays (DDOs). The new Ministerial Direction has, among other things, placed greater emphasis on ensuring consistency between strategy plans and planning schemes, i.e. that the provisions in the planning scheme directly relate back to the source of those provisions, in this instance being the UDF Addendum.



10. SUSTAINABLE DEVELOPMENT DIRECTORATE

10.3 Adoption of Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017 (continued)

Additionally, some of the submitters have advised that they want further clarification around the gateway sites than what the document (adopted in June) provides for. It provides greater clarity and direction to also make these minor changes before exhibition.

The UDF Addendum has now been revised to align with this new Ministerial Direction. It is recommended that Council adopt the revised *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017* (see Attachment 2 - *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017*) and prepare and exhibit a planning scheme amendment to implement its recommendations.

Council Plan/Key Strategic Activity

Strategic Goal 4: *Quality Infrastructure (Built)*

Objective 4.3: Encourage sustainable municipal growth and development

Aim: Greater Shepparton City Council, as one of Australia's fastest growing inland regional cities, recognises that it is important to manage growth in a structured and sustainable manner.

In consultation with the Victorian Government and community stakeholders, we will continue to develop and planning framework that ensures our growth and development does not compromise our enviable lifestyle.

Risk Management

Amendment VC133 was undertaken by the Minister for Planning and requires all planning scheme amendment documentation to align with a new *Ministerial Direction on The Form and Content of Planning Schemes* issued under Section 7(5) of the *Planning and Environment Act 1987*.

Council officers cannot prepare and exhibit a planning scheme amendment that does not align with this new Ministerial Direction. As a result, the UDF Addendum has now been revised to align with this new Ministerial Direction. It is recommended that Council adopt the revised UDF Addendum, and prepare and exhibit a planning scheme amendment to implement its recommendations.

Policy Considerations

The consultation associated with the UDF Addendum was undertaken in accordance with Council's Community Engagement Strategy.

The UDF Addendum supports existing Council policy including the *Greater Shepparton City Council: Council Plan 2013-2017*.

The UDF Addendum now aligns with the new *Ministerial Direction on The Form and Content of Planning Schemes* issued under Section 7(5) of the *Planning and Environment Act 1987*.

Financial Implications

The recommendation will result in financial implications for Council to implement the findings and recommendations of the UDF Addendum. This includes the statutory costs associated with the planning scheme amendment process to implement the Design and Development Overlays into the planning scheme.



10. SUSTAINABLE DEVELOPMENT DIRECTORATE

10.3 Adoption of Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017 (continued)

Legal/Statutory Implications

All procedures associated with the UDF Addendum comply with the legislative requirements. The UDF Addendum has been prepared in accordance with the Greater Shepparton Planning Scheme and Council's Community Engagement Strategy.

The UDF Addendum now aligns with the new *Ministerial Direction on The Form and Content of Planning Schemes* issued under Section 7(5) of the *Planning and Environment Act 1987*.

Environmental/Sustainability Impacts

The UDF Addendum will result in positive environmental/sustainability impacts for the municipality. It seeks to ensure the environmental quality of the landscape is not adversely affected by inappropriate development. The UDF Addendum is cognisant of the *Draft Greater Shepparton Urban Forest Strategy 2017*, the *Greater Shepparton Street Tree Master Plan 2003*, and the *Landscape Plan Guide 2017* and is complementary to these important documents.

Social Implications

The UDF Addendum will result in positive social outcomes for the municipality. It seeks to ensure the quality of Shepparton's built form is enhanced and improved. It includes clear key design considerations to assist in providing quality urban design in new developments.

Economic Impacts

The UDF Addendum will not result in any negative economic impacts.

Consultation

The UDF Addendum was released for public comment from 24 April to 19 May 2017. A summary of the consultation and changes made as a result of consultation are included in the report tabled at the Ordinary Council Meeting held on 20 June 2017 (see *Attachment 1 – Unconfirmed Minutes Extract from June 2017 Ordinary Council Meeting*).

Officers believe that appropriate consultation has occurred and the matter is now ready for Council consideration.

Strategic Links

a) Greater Shepparton 2030 Strategy

Topic: Settlement and Housing

Theme: Sustainable Design

Objective 1: To develop and enhance a distinctive character for Greater Shepparton.

Strategy 1.3: Support the use of urban design frameworks to identify where and how built form can be used to enhance the impact and perception of the municipality.

Strategy 1.4: Enhance the gateways, boulevards and focal points in the municipality with urban design and landscape master plans.

Strategy 1.7: Control the location, size and scale of advertising signage, especial in key precincts of the CBD and town centres.



10. SUSTAINABLE DEVELOPMENT DIRECTORATE

10.3 Adoption of Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017 (continued)

b) Other strategic links

1. Commercial Activities Centres Strategy, 2015;
2. Draft Greater Shepparton Urban Forest Strategy 2017;
3. Greater Shepparton Street Tree Master Plan 2003;
4. Landscape Plan Guide 2017; and
5. Urban Design Framework: Shepparton North and South Business Areas 2006.

Conclusion

The UDF Addendum was adopted by Council at the Ordinary Council Meeting held on 20 June 2017. Council also resolved to note Council officers' intention to prepare and exhibit a planning scheme amendment to implement the recommendations of the UDF Addendum into the Greater Shepparton Planning Scheme, including the Design and Development Overlays.

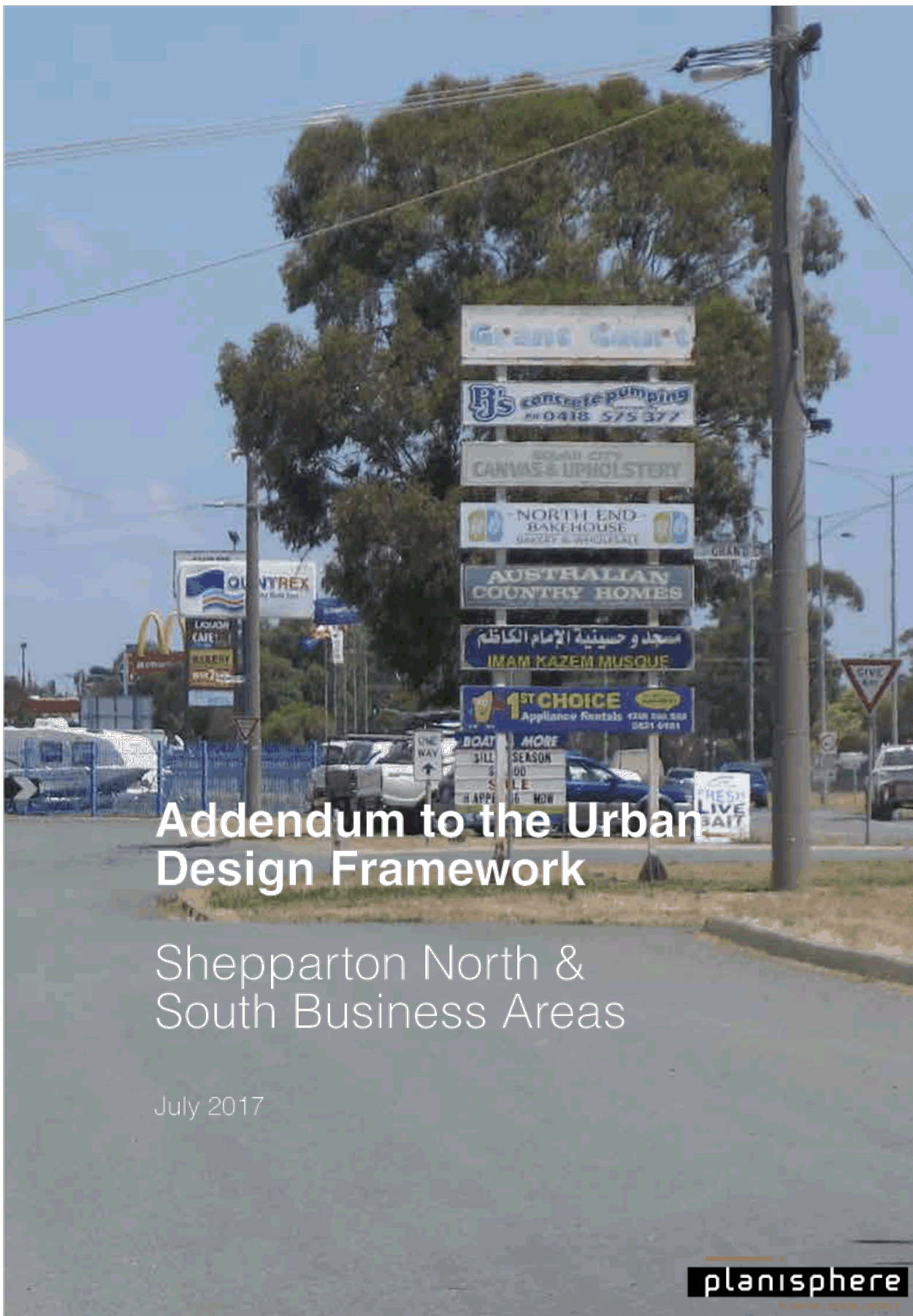
Amendment VC133 was undertaken by the Minister for Planning and requires all planning scheme amendment documentation to align with a new *Ministerial Direction on The Form and Content of Planning Schemes* issued under Section 7(5) of the *Planning and Environment Act 1987*.

The UDF Addendum has now been revised to align with this new Ministerial Direction. It is recommended that Council adopt the revised *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017* (see Attachment 2 - *Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017*) and prepare and exhibit a planning scheme amendment to implement its recommendations.

Attachments

1. Unconfirmed Minutes Extract from June 2017 Ordinary Council Meeting
2. Addendum to the Urban Design Framework: Shepparton North and South Business Areas, July 2017

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Addendum to the Urban Design Framework

Shepparton North & South Business Areas

July 2017

planisphere



Project Control

Status	Version	Checked PM	Checked PD	Date released
Draft Report	3	PH	LR	24.03.2017
Draft Final Report	1	PH	LR	31.03.2017
Draft Final Report	2	PH	LR	26.05.2017
Final Report	1	PH	LR	31.05.2017
Final Report	2	PH	LR	09.06.2017
Final Report	3	PH	LR	28.06.2017
Final Report	4	PH	LR	11.07.2017

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C	Proposed Amendment to DDO7: Kialla Park Boulevard Precinct	



About this Project



Planisphere has been engaged by Greater Shepparton City Council (Council) to prepare a review and addendum to the Shepparton North and South Business Areas Urban Design Framework (2006), prepared by Coomes Consulting.

This document contains the following sections:

- Background review of VCAT decisions relevant to the Urban Design Framework and existing Design and Development Overlays (DDO) affecting the subject areas
- Review of the existing 2006 Urban Design Framework (UDF)
- Review of the contextual analysis and framework for the "Broken River to Raftery Road" area, including the entrance of Kialla Lakes Drive
- Preparation of contextual analysis and framework of the Kialla Lakes Drive entrance and IN3Z area
- Contextual analysis for the commercial land in Shepparton North
- Framework for the commercial land in Shepparton North
- Recommendations (including a new schedule to the DDO for Shepparton North "Gateway Commercial Area" precinct).

The project has been undertaken in two stages:

1. Site survey and initial desktop and background document review.
2. Preparation of the addendum to the UDF, applying the findings of stage 1 and two subject area analyses.

Maps showing the two subject areas have been included in Chapter 1 of this report.



1.0

Introduction & Background Review

1.1 Introduction

2

INTRODUCTION & BACKGROUND REVIEW

This Project

This project seeks to examine and include two additional 'subject areas' to the existing Urban Design Framework: Shepparton North and South Business Areas (UDF). The UDF was prepared by Coomes Consulting in July 2006 and adopted by Council. It has since been used to apply Design and Development Overlay (DDO) schedules to the precincts identified in the study.

Planisphere has been engaged to review and update the UDF to include the following precincts:

- Shepparton North Gateway Commercial Precinct (land covered by the Commercial 1 and Commercial 2 Zone)
- Kialla Lakes Drive area within the Broken River to Raftery Road precinct (The entrance to Kialla Lakes Drive, all land in the Industrial 3 Zone and land in the road reserve between 15-19 Kialla Lakes Drive and 7958 Goulburn Valley Highway and 7950-7954 Kialla Lakes Drive).

Map showing the subject areas have been included on the following pages.

The project also included a review of Schedule 7 to the Design and Development Overlay (DDO) to implement the recommendations of Kialla Lakes Drive.

In addition, recommendations (and changes, as necessary) were made to ensure compliance with Ministerial Direction 13 on the form and content of planning schemes.

Methodology

The following methodology is based on *Planning Practice Note 17 (PPN17): Urban Design Frameworks* (July 2015). The practice note has also been used to underpin a review of the existing UDF (at Section 1.3). A summary of *PPN17* has been included as an appendix to this report.

This project methodology is as follows:

1. Background document review (existing UDF, recent VCAT cases and current planning policy framework)
2. Site visit to review subject area boundaries and existing conditions
3. Contextual analysis based on information gathered in stages 1 and 2
4. Draft framework based on key findings from the contextual analysis
5. Draft recommendations and implementation guide based on key findings from stage 4
6. Public exhibition, consultation with Council and workshops to review draft framework and recommendations

7. Amend document to reflect any changes from public submissions or Council comments

8. Final report.

Subject Areas

The Shepparton North subject area was selected due to changes and development pressures not envisaged by the 2006 UDF, particularly in relation to large advertising signage. The Kialla Lakes Drive subject area was selected due to a change in the road alignment and the interface issues emerging between the adjoining residential area and the existing industrial/commercial development. Boundaries were established using existing zoning controls and as requested by Council:

- All land in the Shepparton North commercial zones
- All land in the Industrial 3 Zone (IN3Z) adjoining Kialla Lakes Drive and the public realm/road reserve forming the entrance to Kialla Parks residential estate.

A site visit was conducted on 20 December, 2016 and the subject area boundaries were confirmed.

Document Structure

This document contains the following sections:

- 1. Introduction and Background Review (this chapter):** Review of recent VCAT cases and the existing Shepparton North and South Business Areas UDF.
- 2. Shepparton North Gateway Commercial Precinct:** Contextual analysis, design framework, conclusions and recommendations.
- 3. Kialla Lakes Drive:** Contextual analysis, design framework, conclusions and recommendations.
- 4. Appendices:** Overview of Urban Design Frameworks Practice Note (A), draft DDO schedule for Shepparton North Gateway Commercial Precinct and draft amendment to the DDO7 to include Kialla Lakes Drive.

Ministerial Direction

It is noted that following issue of the final UDF Addendum document, the proposed DDO schedules for the Shepparton North Commercial Gateway Precinct and the Kialla Lakes Drive Precinct have been updated. The changes were made to reflect the requirements of the *Ministerial Direction on the Form and Content of Planning Schemes* gazetted by the Minister for Planning on 24 May 2017.

1.2 Subject Areas

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Figure 1. Shepparton North Gateway Commercial Precinct

INTRODUCTION & BACKGROUND REVIEW

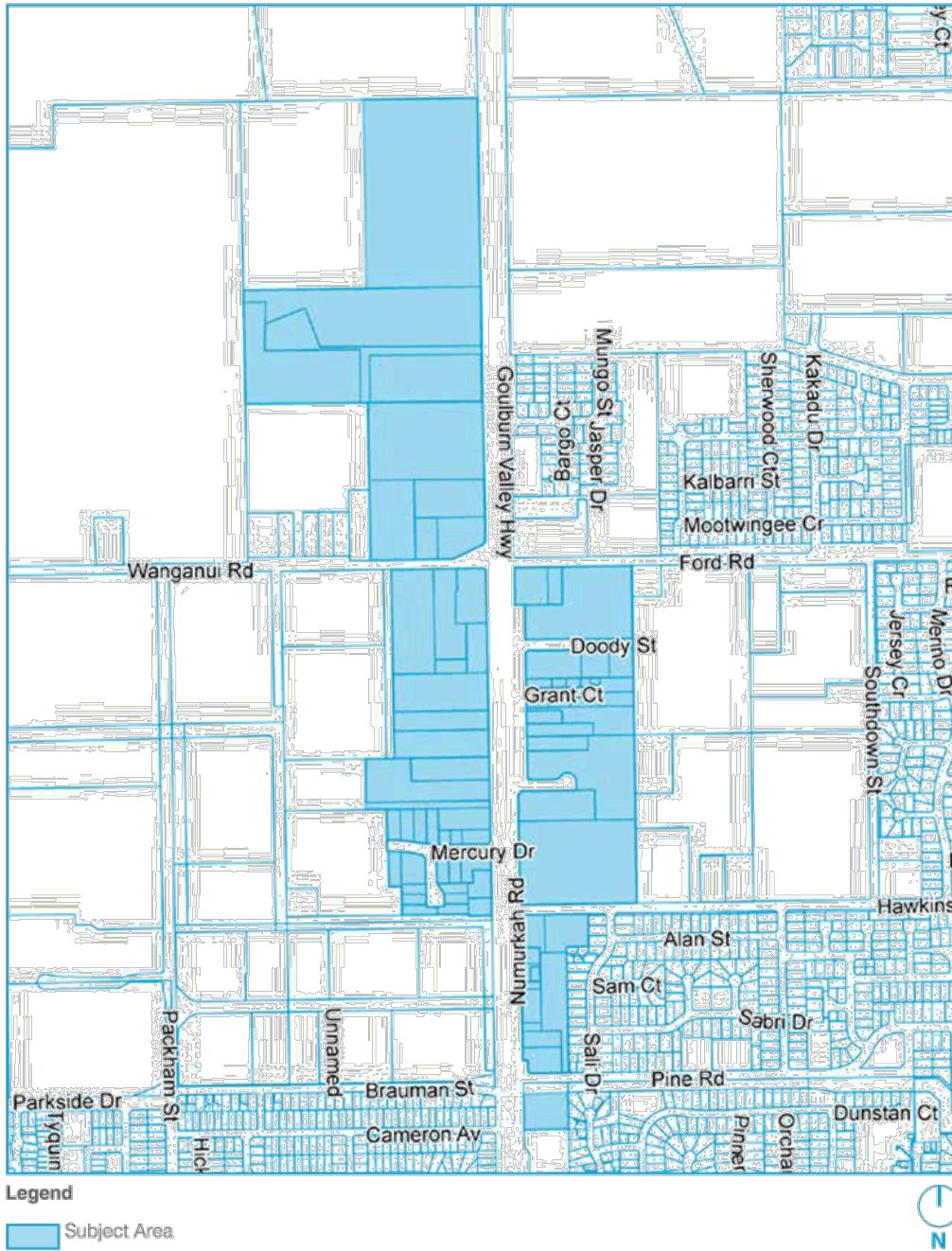
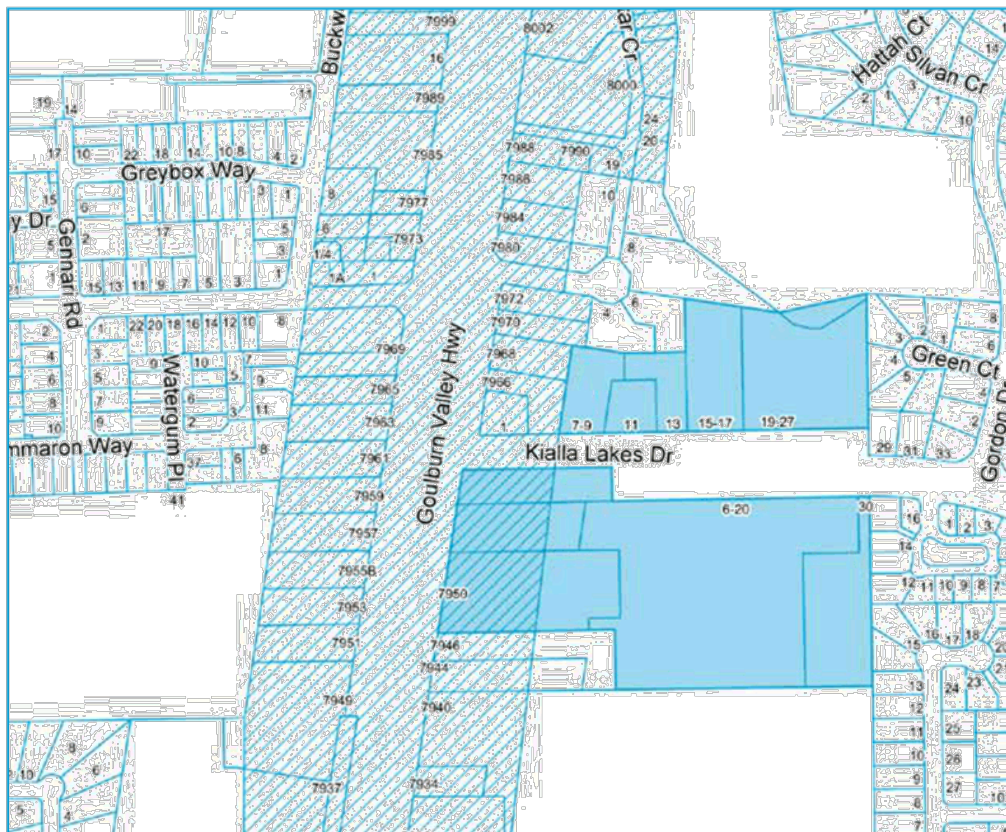


Figure 2. Kialla Lakes Drive



Legend

- Subject Area
- Existing Kialla Lakes Park Precinct



1.3 Review of Existing Urban Design Framework

6

INTRODUCTION & BACKGROUND REVIEW

Broken River to Raftery Road Precinct

The Broken River to Raftery Road Precinct (Kialla Park Boulevard Framework) is located to the south of the Shepparton township and extends from Broken River Drive (north), to Raftery Road (south). Two gateway sites are identified in the UDF:

- Intersection of Kialla Lakes Drive and Goulburn Valley Highway
- Intersection of Raftery Road and Goulburn Valley Highway (GVH).

As highlighted at the start of this report, development pressures in the Shepparton North C2Z area and road realignment in the Kialla Lakes areas have emerged since the 2006 UDF.

Addressing the Shepparton North area requires the creation of a new precinct.

Addressing the Kialla Lakes Drive entrance and Industrial 3 Zone (IN3Z) land results in an extension of the existing precinct (as illustrated in the map on p. 4) along Kialla Lakes Drive from the GVH corridor.

The box at right contains an extract from the existing UDF (2006) relevant to the current study areas.

Extract from the Urban Design Framework: Shepparton North and South Business Areas (UDF), July 2006.

Contextual Analysis:

- *The GVH landscape gateway treatment is a key feature signifying arrival to Shepparton city*
- *Existing character is large big box service businesses comprising bulky goods, retail, manufacturing plants, auto-related services, home making and other eclectic uses*
- *Consistent front setbacks and building scale along GVH*
- *Landscape maintenance and tree removal issues along GVH*
- *Entrances to the residential developments behind the business/commercial strip are not well defined*
- *This precinct lies in the floodplain of the Goulburn River and is within the LSIO (Land Subject to Inundation Overlay)*
- *Substantial new residential areas behind the commercial strips are accessed off the GVH and these access points will need to be clearly legible.*

Vision:

- *Provide robust, suitably scaled landscape treatments to integrate the built form, provide an appropriate scale, reduce its visual impact and provide a suitable interface between the commercial and the residential areas.*
- *Provide clearly defined, legible and attractive gateways to the residential areas behind the commercial activity areas.*

Framework:

- *Clearly defined, legible and attractive gateways along the highway to the residential areas behind the business activity areas.*
- *Innovative landscape treatments and urban art to create clear landscapes and distinctive gateways.*
- *Development sites at these locations should consider innovative built form and landscape treatments to signify these gateways, provide landmarks and reinforce the intersections.*

Shepparton Town Entry - North Precinct

The 'Balaclava Road to Knight Street' area is currently the northern extent of the UDF, known as the 'Shepparton Town Entry - North' precinct.

The contextual analysis for this precinct highlights a need to protect the area as a key gateway for the Shepparton township, particularly where the road narrows to create a sense of arrival into the 'town area' (UDF, p. 10).

In the existing UDF (2006), the precinct is described as follows.

Extract from the Urban Design Framework: Shepparton North and South Business Areas (UDF), July 2006.

- *Fine grained urban character defined by residential houses and strong residential character*
- *Wyndham St road reserve north of Balaclava Rd is large with a diffused built edge; the reserve narrows to south of Balaclava Rd creating a visual 'funnel' effect.*
- *Residential character threatened by newer, larger scale development (residential and commercial) on Wyndham St out of keeping with existing character*
- *Signage, blank façades and 'gun barrel' driveways have started to dominate the streetscape.*

Shepparton North Gateway Commercial Precinct

The Shepparton North Gateway Commercial Precinct is located to the north of the Town Entry - North Precinct. This area roughly covers land between Wanganui Rd (north) and Pine Rd (south). Adding this area in the UDF will extend the land to which the strategy applies, and some amendments to the Town Entry - North Precinct are required:

- Revise description and vision to reflect the amended status of the precinct as an entrance to the town
- Update maps where required
- Retain gateway site identification but amend description to define its role in relation to the new precinct.

Draft contextual analysis and framework sections for the new Shepparton North Gateway Commercial Precinct are included in Chapter 2 of this report.

Summary of Gaps/ Recommended Changes

The existing Kialla Park Boulevard framework contains design guidelines for development fronting GVH, but lacks built form guidelines to manage the transition from industrial to residential land on Kialla Lakes Drive. Accordingly, the following information should be added

as an addendum to that section of the UDF:

- Contextual analysis for Kialla Lakes Drive area
- Vision, design and development objectives and policy approach, adding a framework to address the residential-industrial transition area on Kialla Lakes Drive For the new precinct (Shepparton North Gateway Commercial Precinct), the following information should be added to the UDF:
- Contextual analysis for land in the commercial zones (C1Z and C2Z) north of Pine Rd and at 111-125 Numurkah Road (including identification of any gateway sites)
- Vision, design and development objectives and policy approach added as a new framework chapter to the existing UDF.

In addition, the contextual analysis and framework for the Shepparton Town Entry - North Precinct (Balaclava Road to Knight Street) should be updated as suggested, to acknowledge the importance of Shepparton North C2Z Precinct as a significant gateway/entrance to the town.

1.4 Review of Recent Tribunal Decisions

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INTRODUCTION & BACKGROUND REVIEW

This section outlines two recent Victorian Civil and Administrative Tribunal (VCAT) decisions relevant to this project.

Both cases relate to signage and the existing policy framework in the Commercial 1 and Commercial 2 Zone (C1Z and C2Z) areas north of the town centre.

O.S.O International v Greater Shepparton CC [2016] VCAT 1709 (10 Oct 2016)

Proposal: Erection and display of an externally illuminated major promotion sign (Dimensions: maximum overall height 8.3m, sign 12.66m wide and 3.35m high)

Council's Decision: Refusal

VCAT Decision: Council decision set aside and planning permit granted

This site is located at 195 Numurkah Road (GVH), Shepparton North, within the subject area for the Shepparton North C2Z. Council's decision to refuse the proposal was based on the size and location of the sign.

Key considerations in the Tribunal's determination were as follows:

- The site is not affected by a Design and Development Overlay and has not been identified as being located in a 'gateway' location requiring more stringent controls

- The site is located in a commercial, highway corridor and is not surrounded by any sensitive uses, making it suitable for the display of a major promotion sign
- There are no significant streetscapes, buildings, skylines, view corridors, gateways, landmarks, heritage places, public open spaces or waterways in this locality which need protecting
- The proposal is in line with decision guidelines at Clause 52.05-6 relating to the appropriateness of a major promotion sign
- No 'strong built form character' has been identified in this area, limiting consideration of the sign's impact on existing character
- Due to the building's significant setback, it is inappropriate to locate the proposed sign on the wall of the building as passing motorists would have difficulty reading it.

Popelier v Greater Shepparton CC [2016] VCAT 1228 (21 July 2016)

Proposal: Erection of an internally illuminated (electronic) promotion sign (Dimensions: sign 4.2m wide and 2.4m high, 6.6m above the ground on the building facade)

Council's Decision: Refusal

VCAT Decision: Council decision upheld and no planning permit granted

The subject site is located on the north-west corner of Wyndham St and Knight St, within the Shepparton Town Entry - North precinct. It is in a Commercial 1 Zone (C1Z) and covered by a Design and Development Overlay (DDO3).

The Tribunal's determination to was based on the following:

- The site is within the Shepparton Town Entry - North Precinct (DDO3) and the proposed sign on the corner of this site would be contrary to the preferred signage in the precinct north of Knight Street
- Promotion signs in this location are specifically discouraged
- While the proposed promotion sign is relatively modest and would fit well into the building facade, it would be contrary to the policy framework and prevailing character of the area.

Key Findings

The Tribunal's determinations in both *O.S.O. International v Greater Shepparton CC* and *Popelier v Greater Shepparton CC* show that:

- Greater guidance is needed in the planning policy framework (e.g. identifying local character) in the Shepparton North C2Z area to achieve desired signage, landscape and built form outcomes.
- Although Clause 21.04-4 (Urban Design) has been used to manage signage size, location and design within the DDO3 area, more detailed guidelines may be needed to ensure the purpose of the C1Z and C2Z as they relate to Clause 52.05 (Advertising Signs) are not contradicted. The commercial zones enable the least restrictive signage design requirements due to an assumption of fewer surrounding amenity concerns, particularly in the C2Z. Therefore where amenity/streetscape concerns exist these need definition and protection through other controls.

- The C2Z area in Shepparton North currently does not have a control mechanism to engage with urban design policies, meaning the local planning policy cannot be effectively implemented. Where an overlay control exists this is afforded much more weight at appeal.

Conclusion

In summary, the Tribunal's decisions highlight that a clear and robust policy framework identifying local character, and guidelines that set out how discretion is to be used in determining appropriate and inappropriate development in the subject areas is needed. This applies to both the Shepparton North Gateway Commercial Precinct and the Kialla Lakes Drive subject area, which are not included in the current UDF.

10

SHEPPARTON NORTH GATEWAY COMMERCIAL FRECHINCT

2.0

Shepparton North Gateway
Commercial Precinct

2.1 Contextual Analysis

12

Existing Character: Urban Form

SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

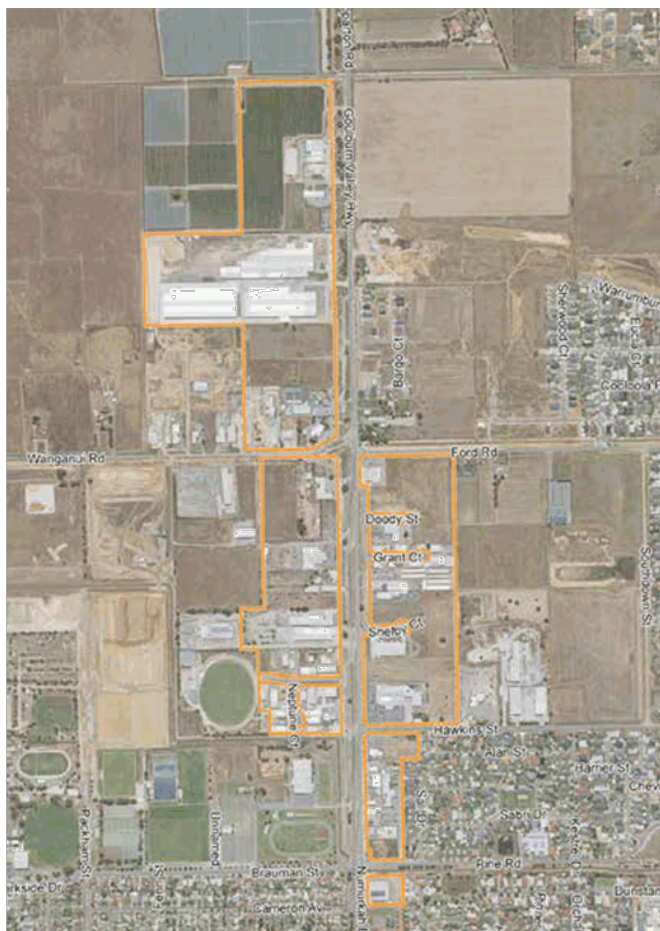
The Shepparton North Gateway Commercial Precinct is centred on the Goulburn Valley Highway (GVH) corridor at the northern entrance to the town.

The existing coarse grained urban character of this area is defined by 'big box' agricultural supplies and large scale commercial buildings with hard stand areas and warehouse-type built form. Recent developments along the southern part of this subject area on Numurkah Road/Goulburn Valley Highway are defined by smaller lot sizes and narrower front setbacks than established sites.

Building scale is predominantly one to two storeys in height with an appearance of one storey, typical of warehouse development.

A median strip separates development from the main Goulburn Valley Highway, creating a service road to access commercial properties. This median strip is often occupied with blade and pole signs bearing business identification information. Brand promotion signage is common on building façades and signage.

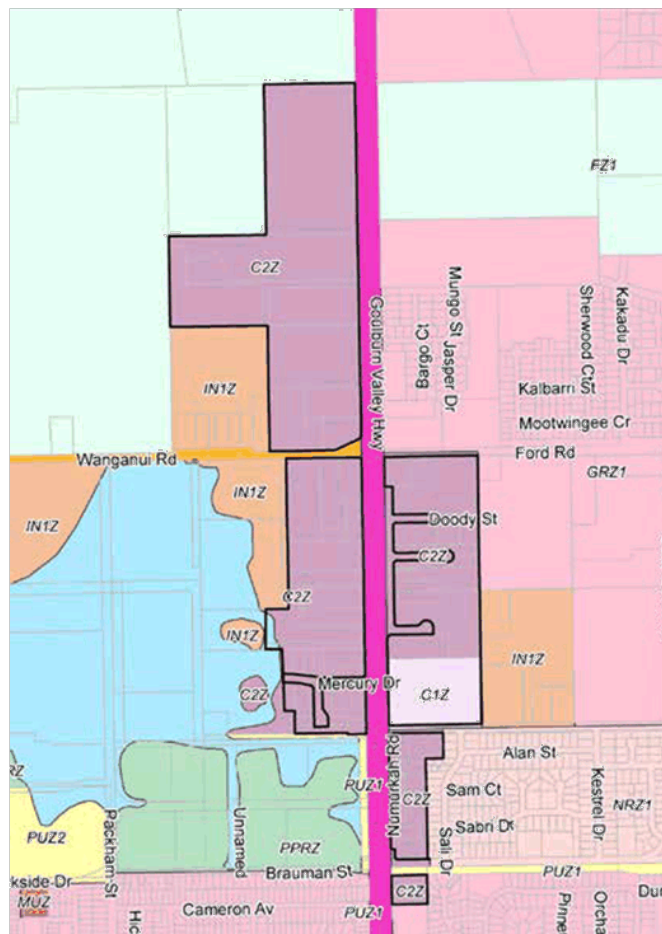
Due to the combined width of the road reserve, setbacks and low scale of development, views through this area and along the road corridor are vast, with big skies and views between buildings to lots behind.



Existing Character: Planning Zones

Legend

-  Subject Area
-  Commercial 1 Zone (C1Z)
-  Commercial 2 Zone (C2Z)
-  Mixed Use Zone (MUZ)
-  Industrial 1 Zone (IN1Z)
-  Urban Floodway Zone (UFZ)
-  Public Use Zone (PUZ2 - Education)
-  General Residential Zone (GRZ1)
-  Neighbourhood Residential Zone (NRZ1)
-  Road Zone, Category 1 (RDZ1)
-  Road Zone, Category 2 (RDZ2)
-  Farming Zone (FZ)



SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

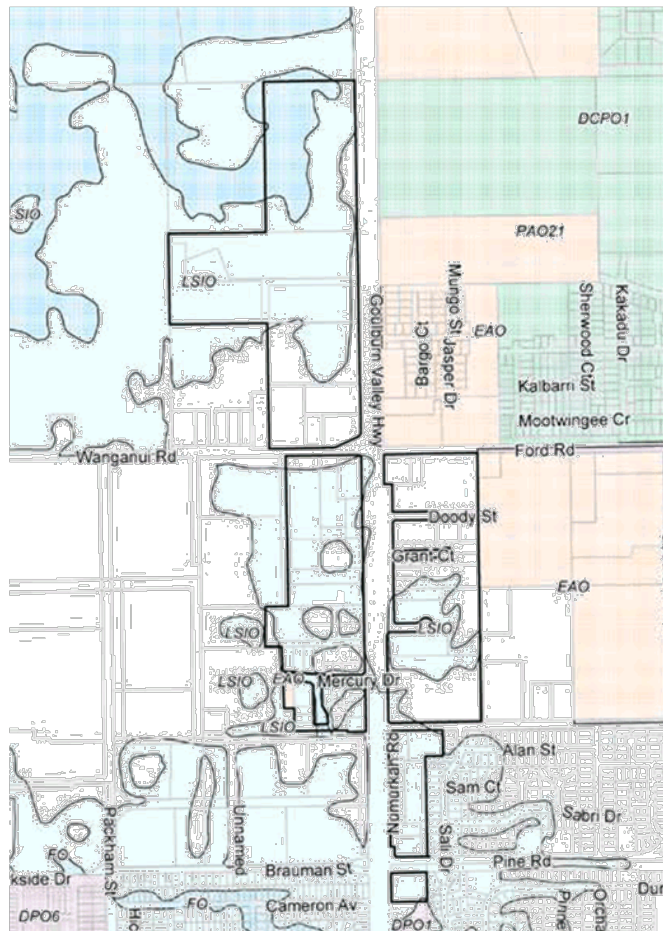


Existing Character: Planning Overlays

SHEPPARTON NORTH GATEWAY COMMERCIAL FRECINCT

Legend

-  Subject Area
-  Development Contributions Plan Overlay (DCPO1 - North Corridor)
-  Development Plan Overlay (DPO1)
-  Development Plan Overlay (DPO6 - Parkside Estate Development Plan)
-  Environmental Audit Overlay (EAO)
-  Floodway Overlay (FO)
-  Land Subject to Inundation Overlay (LSIO)
-  Public Acquisition Overlay (PAO21 - 420A Goulburn Valley Highway, Shepparton - stormwater management)



Existing Character: Predominant Features, Values and Strengths

This stretch of Goulburn Valley Highway/Numurkah Road between Nos. 443 and 111-125 Goulburn Valley Highway, Shepparton North, has a strong semi-industrial and commercial character, created by the warehouse-style big box development with flat roofs, large 'showroom' window proportions and minimal landscaping.

Intermittent business identification signage displayed on pole or blade signs in the front setback or the median strip is common in this precinct, and reinforces the commercial nature of the area.

The street edges are defined by fencing, and some landscaping. Most landscape features are seen in the public realm; particularly street trees planted in the central median strip of Goulburn Valley Highway.

A small number of contemporary commercial buildings with narrower front setbacks and allotments, smaller lot sizes and attached development are found to the south of the subject area. Some of these buildings respect the existing character in terms of building height, maintaining 1 storey and providing some space for landscaping. However, the rhythm and setbacks of this development are distinct from the prevailing character.



Some subdivisions are designed around courtbowls off the service road on the west of GVH. Development in these streets is finer grain, with hard stand car parking and rollover kerbs immediately adjacent to the road reserve. These sites have little to no planted landscape areas and front fencing is uncommon.

Issues and Threats

Despite the grand scale of the Goulburn Valley Highway and service road reserve and three median strips, parts of the corridor are poorly maintained and the avenue-like qualities of this gateway entrance to the town are not realised to their full potential.

In consultation with local business owners, the 'neglected' appearance of the northern gateway was also highlighted. Upgrades to the public realm were underlined as a key consideration that could improve the overall appearance of the area.

Some of the newer commercial buildings along the street display a finer grain character, with limited space between and around buildings.

Hardstand parking areas dominate street frontages, particularly in newer development, and space has not been retained for significant planting (e.g. large trees).

Façades are often visually cluttered with a combination of advertising (e.g. brand promotion) and business identification signage.

Front setbacks and median strips are dominated by advertising and

business identification signage that is not uniformly designed or displayed, creating a corridor of visual clutter when entering the town from the north.

Poorly defined landscape areas in the median strip between the service road and Goulburn Valley Highway has resulted in use of these areas for car parking and display of informal signs (e.g. sandwich boards) or goods for sale (e.g. boats, tractors).

Private realm characterised by service areas and opportunistic signage (including in the median strip between the service road and GVH).

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SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT



Commercial character



Disorganised advertising, road and directional signage: visual clutter with limited visual hierarchy for road users



Car parking and signage clutter in service road median strip



Limited landscaping in front setbacks on Goulburn Valley Highway



No landscaping in front setback on side roads



Signage and products displayed adjacent to the road reserve



Disorganised advertising, road and directional signage

Contextual Influences

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The strong characteristics and influences which define the vision and future character of the precinct are:

- Important location as a gateway to Shepparton when travelling from the north, transitioning between an open rural landscape to the urban area of the town.
 - The very wide road reserve of Goulburn Valley Highway and adjoining service roads is expansive, creating a broad viewing corridor towards the town centre (to the south) and farming land (to the north).
 - Spacious and expansive views to the surrounding landscape through gaps in development and space around buildings, particularly in the northern part of the precinct.
 - Although highway corridors are often experienced at high speed, this section of Goulburn Valley Highway has a 60km/hour speed restriction. Coupled with the length (over 2km) of the road within the subject area, this section of the highway is experienced over a longer period than usual for a highway corridor when travelling through. The visual impact of development, landscape treatment and signage in this area is therefore significant as drivers pass slowly through the precinct.
- Large scale, low level commercial land uses that provide services and goods for the local and wider region.
 - Intermittent provision and inconsistent styles of landscaping.
 - Transition at the northern end of the precinct from productive agricultural character to commercial uses servicing agricultural needs.
 - Transition at the southern end of the precinct from large lot, coarse grain development and agricultural commercial businesses to finer grain development and more town-focussed commercial businesses.
 - Some sites within the precinct have strategic importance due to having greater visual prominence, for example corner sites and those at the northern and southern extent of the precinct.
 - GVH is a VicRoads managed road, therefore all signage must comply with the relevant guidelines.



SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

Vision, Objectives and Strategies

This section contains a vision, objectives and strategies relevant to preparing a planning policy and design framework for the Shepparton North Gateway Commercial Precinct.

Vision: 'Shepparton North Gateway Commercial Precinct'

Encourage a vibrant commercial area supporting large scale, regionally significant service and retail activities, within a high quality public realm that reflects the strategic importance of the area as a gateway location.

- Ensure the design, quality and siting of buildings reflects the gateway role of the area.
- Encourage innovative, high quality design and efficient use of land, while enhancing the appearance and strengthening the identity of the commercial corridor. This includes gateway sites, prominent corners, and sites at the northern and southern extents of the precinct.
- Ensure signage is appropriately designed, sited and scaled for the highway corridor context and minimises visual clutter.
- Maintain a sense of openness and views to the surrounding landscape corridor from the Goulburn Valley Highway.
- Integrate public realm planting and building design with the surrounding landscape.
- Ensure proposed landscaping and plant selection complies with Council's Landscape Plan Guide (Spiire, 2017) and any relevant planting schedule.

2.2 Framework

Gateways

It is noted that there are a number of 'prominent' sites (including gateways) in this precinct, as indicated on the plan. In the case of applications for these designated gateway sites, greater flexibility may be afforded, providing it can be demonstrated that:

- The design objectives for the precinct are achieved;
- Development will be of an exemplary quality design that would make a positive contribution to the character of the precinct; and
- Any variation to the recommended height and/ or setback requirements will have clear design benefits and will not have an adverse amenity impact on the public realm or amenity of adjoining properties; particularly where the precinct abuts land in a residential zone.

Objectives

- Encourage gateway features at specific localities within the precinct to signify entrance points to central Shepparton and the 'Town Entry North' precinct.
- Encourage high quality architectural and design outcomes that elevate the appearance of the precinct and engage with the important entry and land use transition roles of this precinct.

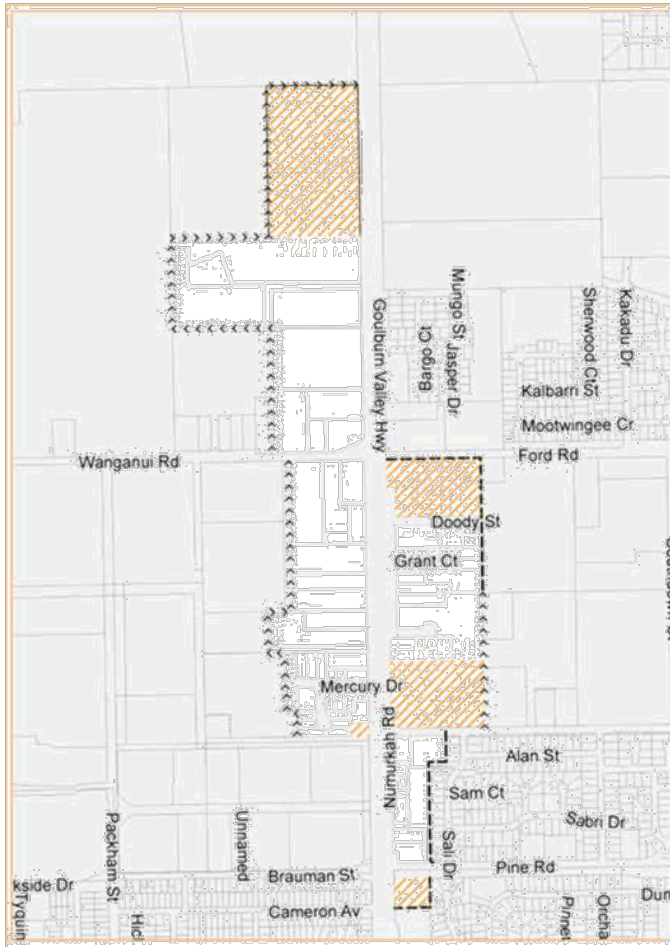
Design Requirements

- Development on the northern edge of the precinct, prominent corners and sites, should incorporate innovative designs using form, materials and articulation to reflect the transition from rural to urban environment.
- Development on the southern edge of the precinct should be stepped down using innovative and high quality architectural techniques to acknowledge the transition from large scale commercial development to the finer grain, residential and mixed use 'Shepparton Town Entry North' precinct.

Refer to the 'Framework Plan' on the following page for further information.

Shepparton North Commercial Gateway Precinct: Framework Plan

SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

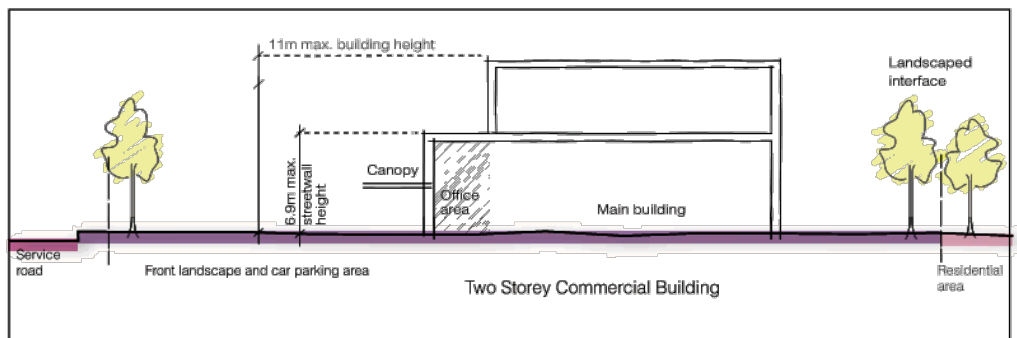


Legend

- Land outside subject area
- Prominent sites (including gateways)
- Property boundaries
- Interface with adjoining residential zoned land
- >>>> Interface with other non-commercially-zoned land



Massing and Height



SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

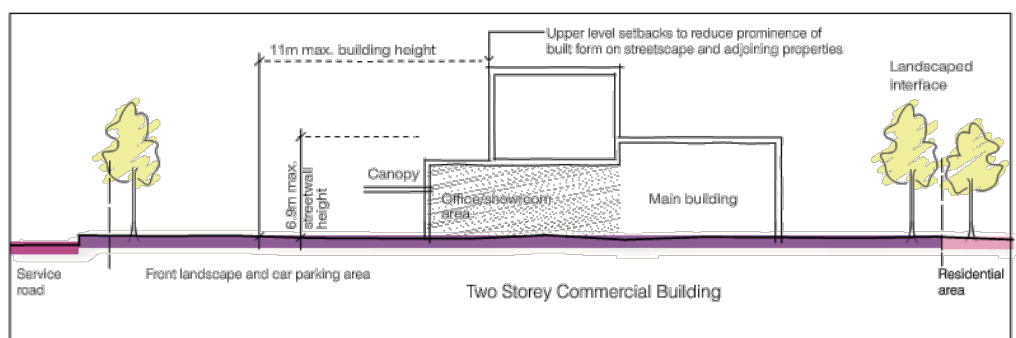
Objectives

- To ensure the prevailing scale of the large format commercial precinct is retained.
- To avoid poorly articulated buildings that lack orientation and integration with the street frontage.
- To provide for building height that is appropriate to the scale of the area, while maintaining opportunities for views to the surrounding landscape.

Design Requirements

- The size of new commercial or semi-industrial buildings should be in keeping with the large format scale of surrounding development.
- Built form should be articulated to avoid blank and dominant façades and create an appropriate transition between pedestrian entries, offices, and warehouse areas.
- Buildings should not exceed two storeys or 11m above natural ground level.
- Offices or building areas open to the public should be located at the front of the building massing.

Street Wall Height and Upper Building Setbacks



Objectives

- To ensure that building mass does not adversely impact on the streetscape.
- To provide a human scale to the street frontage and building entries.
- Where relevant, to ensure built form maintains a scale suitable for the precinct as a town entry point and maximises opportunities afforded by the highway frontage.

Design Requirements

- The street wall height of a building frontage should not exceed 6.9m above natural ground level.
- Provide upper level setbacks that reduce building bulk and add visual interest.

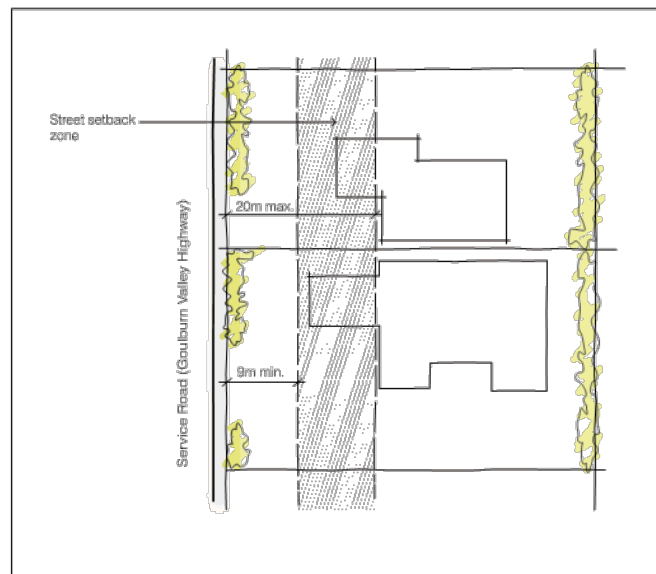
Street Setbacks

Objectives

- To encourage all development to enhance the streetscape character by reinforcing a consistent street setback.
- To ensure street setbacks contribute to efficient use of the site.

Design Requirements

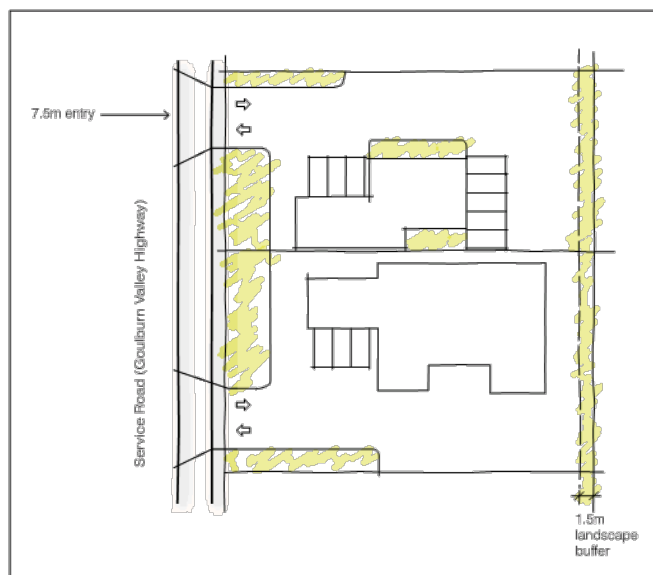
- Street setbacks should be a minimum of 20 metres from the front lot boundary. A lesser setback of up to 9m may be considered to maximise efficient use of smaller sites.
- The above setback excludes a part of the building set back from the front building facade for the purpose of loading/unloading vehicles. This area should be set back greater than 20m from the front property boundary.
- Street setbacks must incorporate significant landscaping and sealed car parking and be utilised to provide clear sightlines and legibility to the street.



Side and Rear Setbacks

Objectives

- To ensure an appropriate interface is provided between new developments and properties in the Commercial 1 Zone (C1Z), Farming Zone (FZ), Industrial Zone IN1Z, Public Park and Recreation Zone (PPRZ), Urban Floodway Zone (UFZ) and General Residential Zone (GRZ1) that abut the precinct.
- To limit the potential for land use conflicts between new development in the subject area and adjoining residential areas (GRZ1) that contain dwellings or other sensitive uses.



Design Requirements

- A minimum 1.5m landscape buffer must be established along rear boundaries of commercial/ industrial lots abutting properties in the GRZ1.
- A minimum 1.5m landscape buffer should be provided along side/ rear boundaries abutting any other zone to soften and screen interfaces.
- On land adjacent to the GRZ1, the provisions of Clause 55 must be considered with regard to overshadowing and overlooking.
- Where possible, development should be set off both side boundaries to maximise views from GVH to the surrounding area from between buildings.

Roof Form and Roofline

Objectives

- To ensure that roof forms are an integral and considered aspect of the building design.
- To encourage building design that is climatically appropriate for the local context.

Design Requirements

- Building roof form and pitch (including flat roofs if relevant) should be designed to complement prevailing character.
- Plant equipment, vents and any other mechanical equipment must be carefully designed and sited or incorporated into the roof design such that it is screened or concealed from the street, surrounding public spaces and buildings.
- Where possible, ensure verandahs and canopies are incorporated into single storey built form components to enhance site amenity and provide shade for employees and visitors to the site.
- Provide sustainable water use in buildings by implementing measures to collect rain water runoff from roof areas. Ensure water storage/tanks are located away from public view, and do not impact on neighbours' visual amenity.

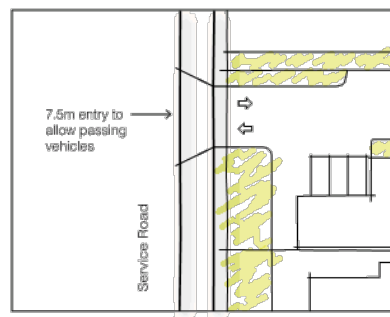
Access Points and Crossovers

Objectives

- To ensure vehicle access to and from the site is safe, manageable and convenient.
- To limit opportunities for conflict in service roads with multiple accessways and access to GVH.
- To maintain an active street frontage that is attractive and accessible.

Design Requirements

- Where practicable, limit vehicle entry points to one crossover and avoid large areas of circular driveway aprons and impermeable surfaces.
- Group access points between properties and limit double crossover widths to large sites.
- For corner sites, encourage vehicle access via a side road rather than the GVH service road.
- Driveway widths should be limited to 7.5m within the front setback area.



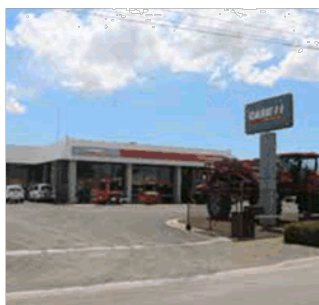
Parking

Objectives

- To provide for the parking needs of building occupants and visitors without adverse impact on the streetscape.
- To avoid parking and traffic difficulties on site.
- To avoid pressure for informal parking within the public realm (e.g. on service road median strips).
- To ensure vehicle parking areas in the front setback are well designed to allow space for landscaping.

Design Requirements

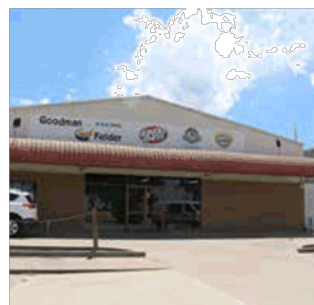
- Sufficient car parking spaces should be provided on site for occupants and visitors in accordance with Clause 52.06 of the Greater Shepparton Planning Scheme.
- For larger sites, short-term parking areas should be located in the front setback, with longer-term parking (i.e. staff parking) located to the rear.
- Avoid large paved car parking areas that are not broken up by landscaping.
- Ensure a landscape buffer is provided between car parking areas and the front property boundary.
- Where practicable, use landscape design to provide significant canopy trees that can assist in shading car parking areas to mitigate heat impacts.



Avoid: Unsealed surface leads to dust and gravel spill



Avoid: Streetscape dominated by hard-stand parking areas poorly designed access arrangements - vehicles must use streetscape to turn around



Avoid: Large concrete parking area dominates the streetscape, and is exaggerated by the lack of planting.

Landscape and Front Gardens

Objectives

- To encourage a front landscape setting that contributes to and enhances the amenity of the GVH corridor as a key town entry point.
- To ensure landscape dominates the streetscape.
- To create consistent planting and opportunities for the establishment of significant trees in both the public and private realm.
- To ensure plant choice is suitable and sustainable within the environment.
- To ensure fencing enhances the street appearance, and maintains views to, between and around sites.

Design Requirements

- A minimum of 15% of the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree (mature height 10m). Plant species must be appropriate and suitable to this area, and to Council's satisfaction.
- Provide large canopy trees and understorey planting to minimise loss of views from the GVH corridor to the surrounding landscape.
- Minimise areas of paving, particularly in the front setback.
- Front fences should have a minimum 50% visual permeability.
- Fencing should be constructed from materials that complement the building and surrounding area and should be painted in muted colours/tonings, avoiding galvanised steel mesh and 'Colorbond' style fencing.
- Wherever possible, fencing should be softened and screened by vegetation.
- Sustainable water use should be achieved using water sensitive urban design (WSUD) principles and low water use plant materials.



Encourage: Space for landscaping within the front setback that uplifts the character of the area



Encourage: Buffer planting along side and rear boundaries, including the planting of large canopy trees



Avoid: Areas of car parking/ hardstand stock display that are not softened by a landscape buffer when viewed from the road

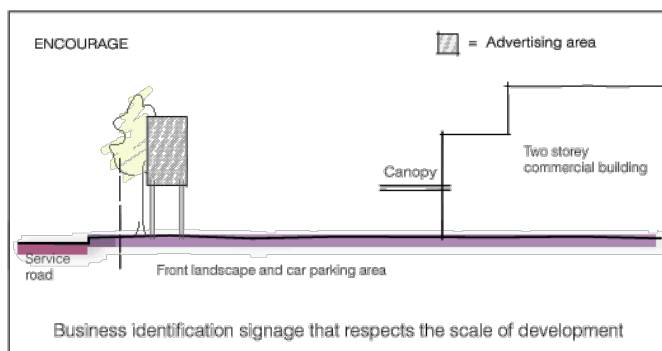
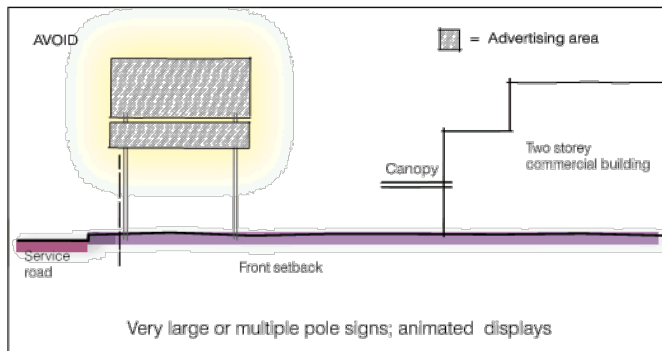
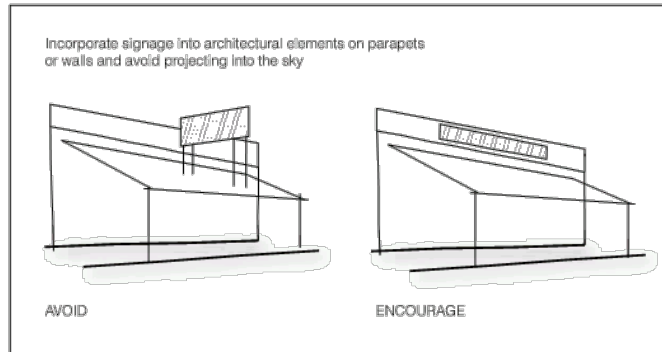
Signage

Objectives

- To ensure signage respects the preferred character of the area.
- To encourage business identification signage that is coordinated and effective.
- To ensure signage does not dominate the site, building on which it is located, streetscape, surrounding landscape and other signage.
- To ensure all way-finding and directional signage is clearly visible and the safe operation of the Highway is prioritised.
- To avoid visual clutter on sites and in the public realm when viewed from the Highway.
- To discourage promotion and major promotion signs.
- To discourage animated and visually distracting signage.



Encourage: Signage that is an integral part of the building facade and has simple, business identification content.



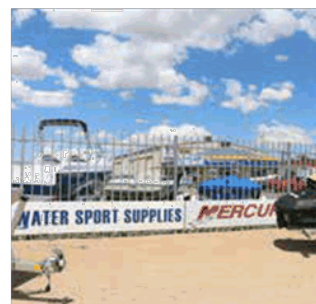
Signage (Continued)

Design Requirements

- Business identification signage should form an integral part of the building facade, appropriately proportioned to sit within the parapet or gable end, and not protrude above or beyond the building façade.
- Business identification information can be incorporated into one way-finding oriented sign at the entrance to each section of service road to reduce visual clutter.
- High wall, panel and sky signs are discouraged.
- Promotion signs that project outside the perimeter of the building, or are free-standing, are discouraged.
- Major promotion signs will only be considered acceptable on a temporary basis (e.g. a maximum of 15 years from the date of permit issue).
- Major promotion signs should be attached to and integrated into the front building facade.
- Free-standing signage (e.g. pole signs) must be set back a minimum distance of 1m from the front property boundary.
- Signage, including painted signs, located on a side wall should not occupy more than 50% of the area of the wall.

- Bunting, animated and reflective signs, including sandwich boards and inflatable and temporary signs, are discouraged.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are animated, contain moving parts or have flashing elements are not encouraged.
- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.

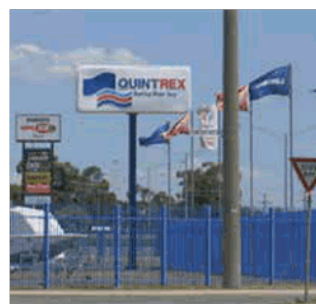
- Refer to Clause 52.05 of the Shepparton Planning Scheme for general requirements relating to signage.



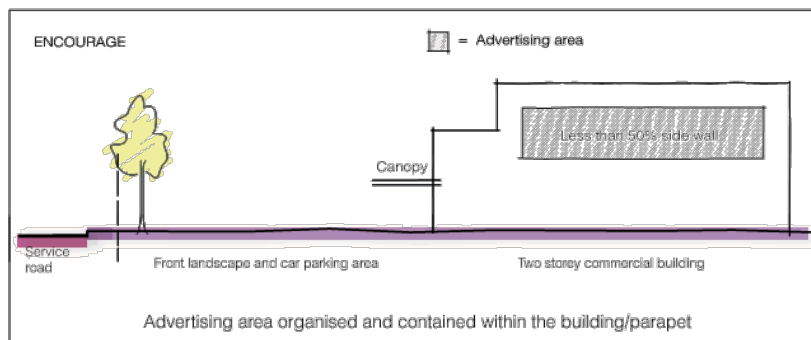
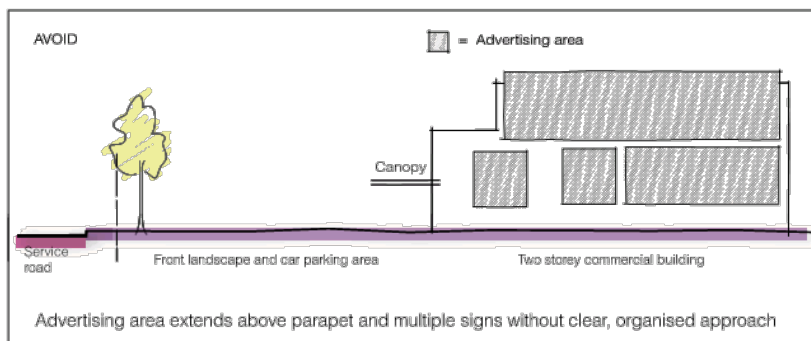
Avoid: Signs scattered across a site without simple messaging and presented in a discordant way.



Avoid: Sandwich board signs and advertising signs are discouraged as they detract from the public realm.



Avoid: Signs that dominate views and the skyline as the most prominent feature of development



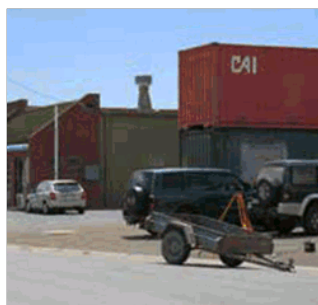
Facade Treatment and Architectural Features

Objectives

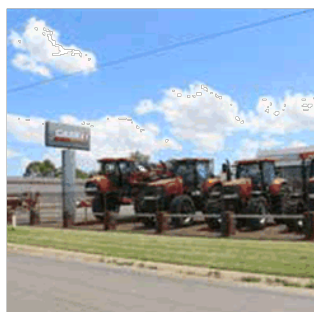
- To maintain and enhance the character of this precinct as a high quality, regionally significant commercial area.
- To ensure built form is of a quality that respects the precinct as an important entrance and gateway site in the north.
- To ensure new buildings contribute to the GVH streetscape in a positive way.

Design Requirements

- Ensure built form treatments are of a large format scale that reflects the highway corridor characteristics of this precinct.
- Provide clearly defined building entrances that are oriented towards the streetscape and have human scale elements.
- Enhance the relationship between built form and the streetscape by providing large window proportions, particularly on the front building elevation.
- Provide articulated building façades that reduce the impact of visual bulk on the streetscape.
- Avoid large, blank walls or uniform façades without articulation.
- New buildings should adopt the appearance of commercial, rather than industrial, built form.
- Buildings, works, plant and machinery should be constructed, housed and maintained in a manner that minimises visual impact.



Avoid: Informal storage areas that dominate the streetscape - these should be constructed, housed and maintained in a manner that minimises visual impact



Encourage: Front fencing to define the street edge and contribute to the streetscape character



Encourage: A front landscape setting to contribute to a preferred streetscape character and public realm



Avoid: Built form that is not articulated and presents a large, blank facade to the streetscape.

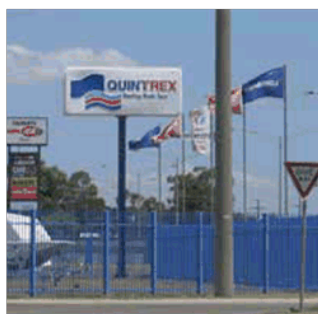
Materials, Colours and Finishes

Objectives

- To ensure that a cohesive streetscape character is achieved through appropriate use of colours and materials.
- To maintain a cohesive streetscape character through use of visually non-intrusive materials, colours and finishes.

Design Requirements

- Materials for external surfaces of new developments should consider and complement those used in existing buildings in the area, such as brick and non-reflective, high quality cladding.
- The use of concrete wall construction (e.g. tilt panels) should be complemented by the use of contrasting claddings such as timber and metals that provide facade definition.
- Reflective glass, PVC, unrelieved painted render, highly reflective materials such as zincalume, unarticulated concrete surfaces and unarticulated cladding systems should be avoided. Bright, extravagant colours schemes should also to be avoided.
- Colour schemes of all external surfaces of new developments must complement those found in the surrounding area and should consider neutral colours.



Avoid: Overly bright colour schemes that do not complement the surrounding area and that specifically stand out in the context of the broader streetscape character



Avoid: Highly reflective colour schemes such as bright whites or zincalume-based finishes

2.3 Summary of Findings

Shepparton North Gateway Commercial Precinct

Summary

The Shepparton North Gateway Commercial Precinct is an important entry point for the town. The area also plays a significant role in providing for agricultural goods that services Greater Shepparton and the wider region.

Accordingly, guidance is needed to manage visual clutter, quality of built form and car parking in the public realm, which currently detract from the area and the important urban design and functional roles it plays.

Recommendations

It is recommended that the Shepparton North Gateway Commercial Precinct Framework within this chapter be used to prepare a Design and Development Overlay (DDO) schedule to address future urban design and development in the area.

A draft DDO has been included in Appendix B of this document. The DDO incorporates the objectives and requirements outlined in the Framework.

It is noted that the draft DDO originally prepared in the previous version of this report has been updated to reflect the requirements of the *Ministerial Direction on the Form and Content of Planning Schemes* (May 2017). The appended DDO schedule is now that updated version.

Further Work

In addition to the recommendations above, it is suggested that Council prepare an advertising signs local policy to support this and other precincts throughout the municipality where variations to the Victoria Planning Provisions (VPP) advertising sign policy (Clause 52.06) are sought.

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SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

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SHEPPARTON NORTH GATEWAY COMMERCIAL FRECHINDT

3.0

Kialla Lakes Drive Addendum

Addendum to the Kialla Park Boulevard Framework

3.1 Contextual Analysis

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KIALLA LAKES DRIVE ADDENDUM

Existing Character: Urban Form

The western portion of Kialla Lakes Drive is defined by a transition from a residential estate to industrial land (Industrial 3 Zone, IN3Z). The road reserve forms a meandering and well-landscaped pathway connecting the residential area to GVH.

The IN3Z lots are characterised by big box service businesses of bulky goods, manufacturing and retail services. Some lots within the IN3Z are vacant.

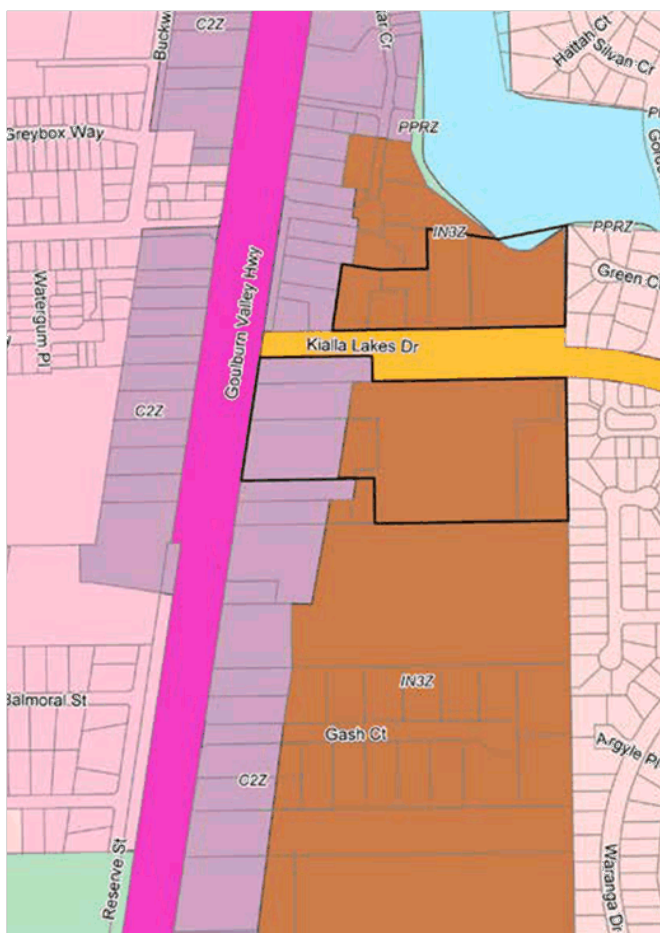
The landscaping along Kialla Lakes Drive is planted in an avenue style on either side of the road and within a wide median strip, creating a high quality public realm at the entrance to Kialla Lakes estate.



Existing Character: Planning Zones

Legend

-  Subject Area
-  Commercial 2 Zone (C2Z)
-  Industrial 2 Zone (IN1Z)
-  Urban Floodway Zone (UFZ)
-  General Residential Zone (GR1)
-  Neighbourhood Residential Zone (NRZ1)
-  Road Zone, Category 1 (RDZ1)
-  Road Zone, Category 2 (RDZ2)
-  Public Park and Recreation Zone (PPRZ)



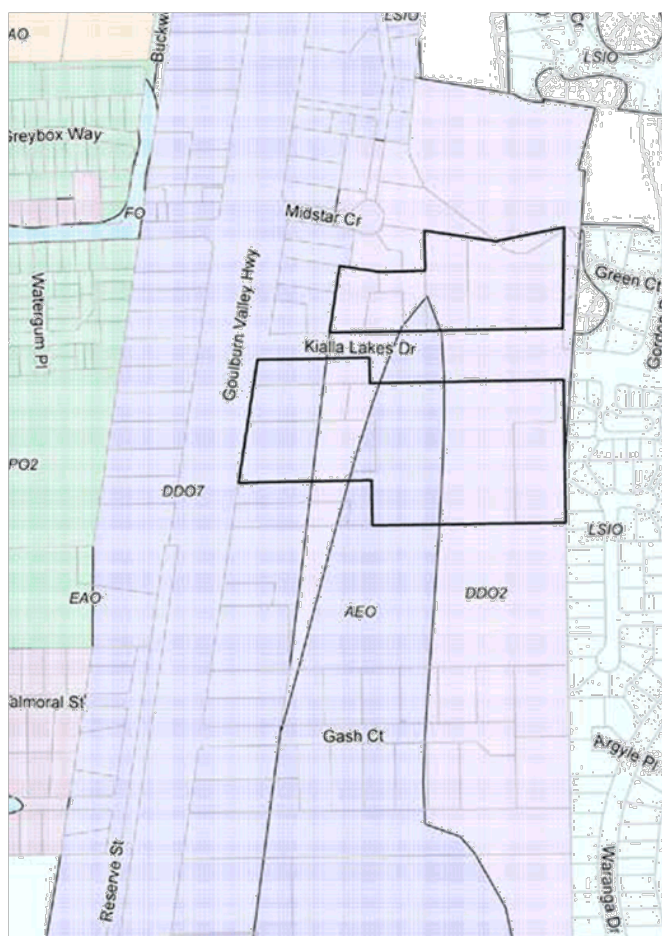
KIALLA LAKES DRIVE ADDENDUM

Existing Character: Planning Overlays

KIALLA LAKES DRIVE ADDENDUM

Legend

-  Subject Area
-  Development Contributions Plan Overlay (DCPO2 - South Corridor)
-  Design and Development Overlay (DDO2 - Airport Environs)
-  Design and Development Overlay (DDO7 - Kialla Park Boulevard Precinct)
-  Development Plan Overlay (DPO3)
-  Environmental Audit Overlay (EAO)
-  Floodway Overlay (FO)
-  Airport Environs Overlay (AEO)



Existing Character: Predominant Features, Values and Strengths

Wide road reserve with generous front setbacks and lots generally set back from the street, accommodating large trees planted in an avenue style. In some parts, trees planted in the front setback of private property enhance this avenue feeling.

The IN3Z appears to be in transition, with a number of vacant lots fenced with post and wire agricultural fencing. One large lot fronting Kialla Lakes Drive is fenced with shipping containers.

There are examples of some big box retailers and manufacturing/warehouse buildings with concrete tilt panel construction and flat roofs. The industrial buildings are generally located within large concrete car parking aprons typical of big box retail precincts in the wider Kialla Park Boulevard precinct.

Residential development adjoining the IN3Z area is characterised by one to two storey dwellings with hipped and tiled roofs and comparatively narrow front setbacks.



Issues and Threats

KIALLA LAKES DRIVE ADDENDUM

Inconsistent treatment of public realm interface between residential and industrial lots.

Scale, siting and design of industrial buildings interrupts view lines and reduces visibility of gateway to residential estate.

Fencing design guidelines in the IN3Z are lacking, with examples of high fencing or shipping containers that create large areas of blank interface with the public realm.

Vacant industrial land adjacent to residential area may be developed without appropriate transition of scale, siting and building design.

Landscaping treatment along the IN3Z portion of the street is inconsistent with the remainder of the street to the residential estate.

There is no clearly defined gateway to the precinct from GVH, meaning the residential area is screened by the existing industrial area.

There is limited integration or transition between existing industrial and residential buildings.

Fence design is inconsistent and poorly maintained in some areas.

Vacant paddocks with a high prevalence of weeds are visible throughout the IN3Z area, creating an unkempt appearance.

Reflective roofing or construction materials in new buildings may have a negative impact within the Airport Environs area within this subject area.



Lack of design guidelines or high quality fencing treatments



Scale and siting of existing industrial uses adjacent to residential area creates interface issues.



Development and hard stand car parking constructed to lot boundaries without provision for landscaping



Unfinished or poorly maintained driveways with gravel spill into the public realm (inappropriate materials)

Contextual Influences

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KIALLA LAKES DRIVE ADDENDUM

The strong characteristics and influences which define the vision and future character of the precinct are:

- New lots created through realignment of Kialla Lakes Drive provide a public realm or new development opportunity in the IN3Z.
- Future development of those two Council-owned areas adjacent to the road reserve (currently vacant lots) will impact on the streetscape and adjoining residential land.
- Emerging light industrial character with big box retail-style development similar to that lining GVH in the Kialla Park Boulevard precinct.
- Substantial new residential areas to the east of the subject area hidden by the scale of development and lack of public realm treatment along the start of Kialla Park Drive to signify an entrance to the estate (particularly from the GVH intersection).
- Curvilinear streetscape winding from IN3Z to the residential area has a partially established boulevard character.
- Kialla Lakes Drive is a key road for accessing the emerging IN3Z area as well as servicing a large area of new residential estates to the east.
- Selected materials, colours and finishes, particularly roofing materials, must be non-reflective to ensure no glare is projected within the Airport Environs area.

Vision, Objectives and Strategies

This section contains a vision, objectives and strategies relevant to preparing a planning policy and design framework for Kialla Lakes Drive.

Vision: Kialla Lakes Drive

Create a cohesive and inviting transition from the industrial precinct (IN3Z) on Goulburn Valley Highway and Kialla Lakes Drive to the Kialla Park residential area. Use design solutions for new industrial development and the public realm to manage this transition; including space for landscaping and high quality fences.

- Ensure the land use transition from IN3Z to residential is managed by a cohesive public realm treatment.
- Encourage new development in the IN3Z that has a sensitive interface with adjoining residential properties (where relevant).
- Provide a clearly defined, legible and attractive gateway from Goulburn Valley Highway to the precinct.
- Encourage innovative building design in industrial areas that respects the gateway nature of this precinct at the transition to the residential estate.

3.2 Framework

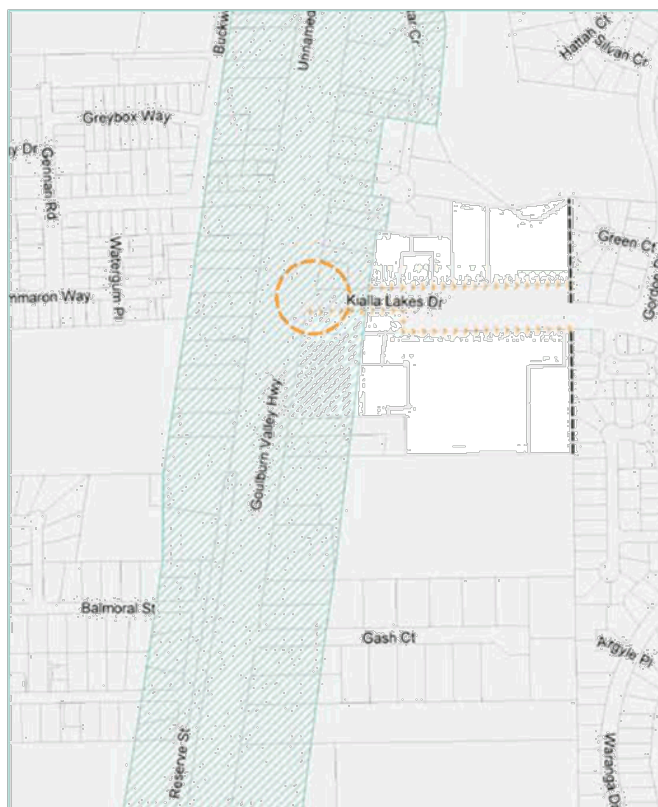
Gateways

Objectives

- To encourage gateway features at the entrance to this precinct that signify the transition from IN3Z to residential area.
- To encourage significant architectural outcomes that reinforce this precinct as a transitional and gateway area.
- To provide a clearly defined, legible and attractive gateway at the intersection of GVH and Kialla Lakes Drive to signify the residential area behind the IN3Z.
- To manage new development within the IN3Z area to ensure high quality of built form and architectural innovation improves the entrance to the residential estate.

Design Requirements

- Developments within the IN3Z should contain innovative built form and landscape treatment that is reflective of this gateway area and extends the boulevard character of Kialla Lakes Drive to better manage the IN3Z-residential interface.
- New development on the Council-owned land must create a transition in scale from the IN3Z lots behind to Kialla Lakes Drive.
- Use gateway treatments to demarcate access from the GVH through the industrial area to the residential properties.

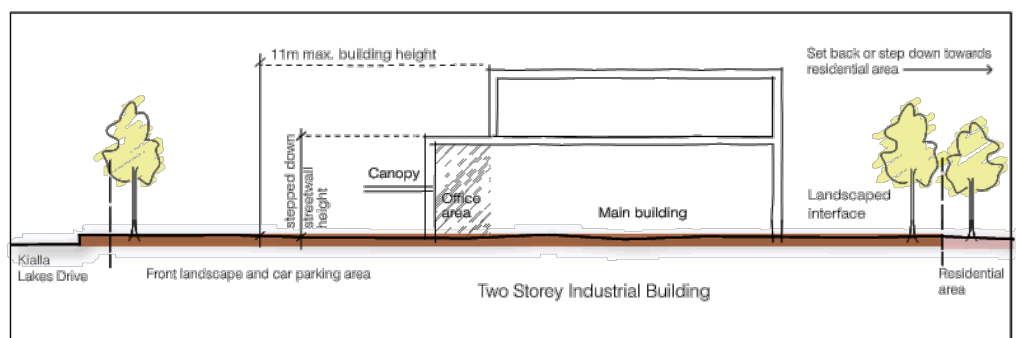


Legend

- Land outside subject area
- Existing Kialla Lakes Park Precinct
- Gateway entrance area
- Property boundaries
- Interface with adjoining residential zoned land
- Interface with adjoining other zoned land



Massing and Height



Note: The Kialla Lakes Drive subject area lies within the floodplain of the Broken River and is covered by a Land Subject to Inundation Overlay (LSIO).

'Finished ground floor level' must be determined by planning provisions of the LSIO schedule, and by requirements of the catchment management authority (CMA).

Objectives

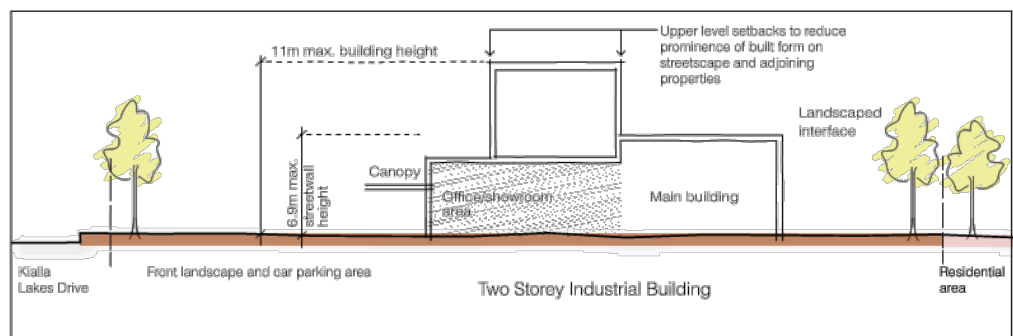
- To ensure that a transition in scale is avoided between the IN3Z and residential areas.
- To avoid large 'box like' industrial buildings with large, blank façades and limited articulation.
- To provide for building height in the IN3Z that does not limit appropriate industrial activities but which is sensitive to the interface with residential properties.

Design Requirements

- The height and form of new industrial or warehouse buildings should be articulated in such a way as to mitigate the appearance of visual bulk in response to the nearby residential fabric.
- Built form articulation should be particularly responsive to streetscape interfaces and be stepped down towards residential interfaces, where those properties share a boundary.
- Building height must be no greater than two storeys within a maximum height of 11m from natural ground level.

Street Wall Height and Upper Building Setbacks

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KIALLA LAKES DRIVE ADDENDUM

Objectives

- To ensure building mass does not adversely impact the Kialla Lakes Drive streetscape or adjoining residential properties.
- To provide a human scale element in the streetscape interface that reduces the visual impact of larger scale built form.
- To ensure a sensitive interface with the surrounding residential neighbourhood.

Design Requirements

- The street wall height of a building frontage must not exceed 6.9m from natural ground level.
- Where two storey built form is proposed, consider varying the upper level setbacks to provide some articulation of the building and reduce dominance of the built form on Kialla Lakes Drive or adjoining residential properties.
- New developments in the IN3Z on lots abutting the NRZ1 must consider overlooking and overshadowing pursuant to Clause 55 of the Greater Shepparton Planning Scheme.

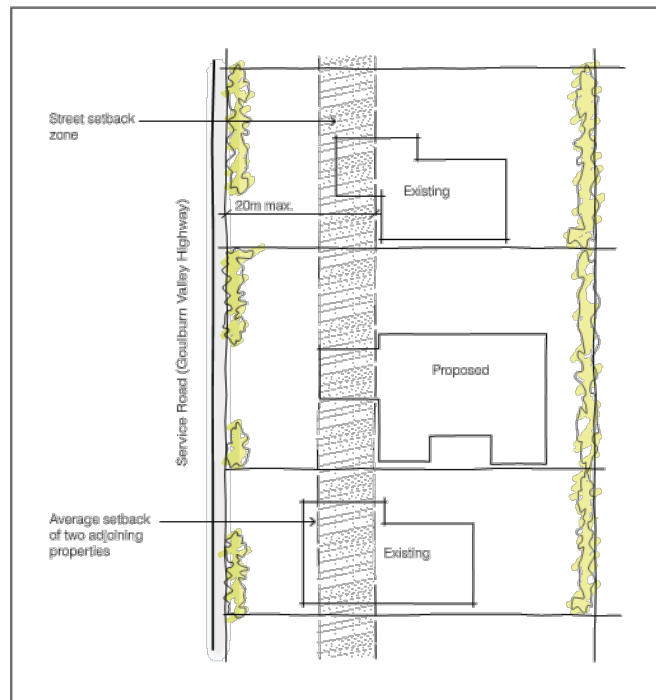
Street Setbacks

Objectives

- To maintain and reinforce consistency in existing street setbacks.
- To ensure street setbacks contribute to efficient use of the site.
- To ensure that street setbacks contribute to an attractive streetscape that reflects the gateway function of this area to the residential area beyond.

Design Requirements

- The front setback of new buildings should be consistent with the setbacks of existing development in the area. The distance for new development in the IN3Z should be taken as an average of the setbacks of adjoining properties in the IN3Z or 20m, whichever is the lesser.
- Street setbacks must consider parking and landscape requirements, and can be utilised to provide short-term parking spaces within the lot frontage.



Side and Rear Setbacks

Objectives

- To ensure an appropriate interface is provided between new developments and residential area to the east.
- To limit the impact of new developments on the amenity of neighbouring buildings and dwellings.
- To provide opportunities for efficient solar access that will address environmentally sustainable site planning.

Design Requirements

- A minimum 1.5m landscape buffer is to be provided along one side or rear boundaries.
- Where possible, development should be set off both side boundaries to limit opportunities for large scale, 'wall like' developments along Kialla Lakes Drive.
- Where a zero (0m) setback is sought on land adjacent to the NRZ1, the provisions of Clause 55 and/or the zone schedule must be applied to overshadowing and overlooking.
- Ensure that long, blank front façades and side façades facing street frontages are avoided and use of glazing is maximised within developments.



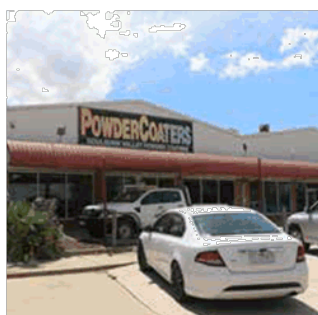
Roof Form and Roofline

Objectives

- To ensure roof forms are an integral and considered aspect of the building design.
- To encourage building design that is climatically appropriate for the local context.

Design Requirements

- Building roof form and pitch (including flat roofs if relevant) should be designed to complement prevailing character. This may include using subtle design cues to reference the nearby residential area.
- Plant equipment, vents and any other mechanical equipment must be carefully designed and sited or incorporated into the roof design such that it is screened or concealed from the street, surrounding spaces and buildings.
- Where possible, ensure verandahs and canopies are incorporated into single storey built form components to enhance site amenity and provide shade for employees and visitors to the site.
- Provide sustainable water use in buildings by implementing measures to collect rain water runoff from roof areas. Ensure water storage/tanks are located away from public view, and do not impact on neighbours' visual amenity.



Encourage: Lower, human-scale entrances featuring articulation/projecting verandah to reduce visual bulk to the streetscape



Avoid: Visible plant and building services on roofs that are not screened from the public realm

Access Points and Crossovers

Objectives

- To ensure the entrance to Kialla Lakes Drive is neatly set out and provides for safe and orderly vehicle movement.
- To limit wide paved areas for site entry/exit that interrupt avenue planting.
- To limit opportunities for conflict between pedestrians accessing the residential area and heavy vehicles or visitors accessing the IN3Z sites.

Design Requirements

- Where practicable, limit vehicle entry points to one crossover and avoid large areas of circular driveway aprons and impermeable surfaces.
- Group access points between properties and limit double crossover widths to large sites.
- Driveways must be designed to minimise any conflict between vehicle movements and pedestrians by maintaining sight lines through use of permeable fencing and appropriate siting of buildings.

Parking

Objectives

- To provide for the parking needs of building occupants and visitors without adverse impact on the streetscape.
- To avoid parking and traffic difficulties on site.
- To avoid informal parking within the public realm (e.g. on service road median strips).
- To ensure vehicle parking areas in the front setback are well designed to allow space for landscaping.

Design Requirements

- Sufficient car parking spaces should be provided on site for occupants and visitors in accordance with Clause 52.06 of the Greater Shepparton Planning Scheme.
- For larger sites, consideration should be given to short-term parking areas located in the front setback, with longer-term parking (i.e. staff parking) located to the rear.
- Avoid large paved car parking areas that are not broken up by landscaping.
- Ensure a landscape buffer is provided between car parking areas and the front property boundary.
- Where practicable, use landscape design to provide significant canopy trees that can assist in shading car parking areas to mitigate heat impacts.

Landscape and Front Gardens

Objectives

- To encourage space for front landscape settings that soften the appearance of 'big box' developments, contribute to and enhance the amenity of Kialla Lakes Drive as an interface between GVH and the NRZ1 residential area to the east.
- To ensure buildings do not significantly dominate the landscape and provide vegetation that is appropriately scaled to address building heights in the IN3Z.
- To create opportunities for consistent planting and establishment of significant trees in both the public and private realm.

Design Requirements

- In the Council-owned areas of road reserve, ensure a sufficiently wide nature strip is retained to facilitate planting of street trees when the land is redeveloped.
- A minimum of 15% of the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree (mature height 10m). Plant species must be appropriate and suitable to this area, and to Council's satisfaction.
- A minimum 3m landscape buffer must be provided along rear boundaries to ensure a suitable interface with residential properties. Where a residential interface abuts a side boundary, a minimum 1.5m landscape buffer must be provided.
- Minimise areas of paving, particularly in the front setback.
- The landscape treatment should serve to soften the 'big box' nature of new development and enhance the appearance of the overall development and streetscape.
- Front fences are encouraged to contribute to a high quality public realm and should have a height no greater than 1.2m and a minimum 50% visual permeability.
- Fencing should be constructed from materials that complement the building and surrounding area and should be painted in muted colours/tonings, avoiding galvanised steel mesh and 'Colorbond' style fencing.
- Wherever possible, fencing should be softened and screened by vegetation.
- Sustainable water use should be achieved using water sensitive urban design (WSUD) principles and low water use plant materials.



Encourage: Front landscape treatments that respond to and incorporate new industrial developments into the boulevard character of Kialla Lakes Drive and the immediately adjoining residential area beyond



Avoid: High front fencing that is visually impermeable, or is constructed from materials that visually detract from the surrounding area

Signage

Objectives

- To provide for business identification signage.
- To provide well designed and integrated signage that contributes to the commercial or semi-industrial activities of the precinct.
- To avoid uncoordinated signage and visual clutter in the public realm.

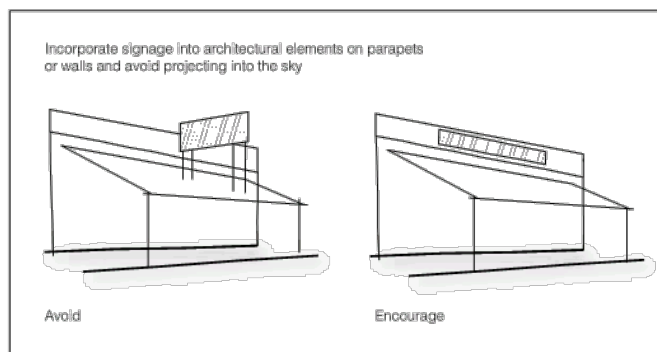
Design Requirements

- One business identification sign is permitted per development.
- Multiple business occupancies are to share space on the sign.
- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2 metres, and a maximum width of 1.5 metres. This envelope includes the height of any supporting structure.

- Signs attached to a building are encouraged and should be a maximum height of 1.0 metre, and a maximum width of 3.0 metres. Exemptions may be made for signs composed of individual letters that form an integral part of the building façade.
- Panel, Promotion and Major Promotion signs should be avoided.
- Above verandah signs including V-board signs and advertising elements such as banners, flags and inflatable should be avoided.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are animated, contain moving parts or have flashing elements are not encouraged.
- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.
- Refer to Clause 52.05 of the Shepparton Planning Scheme for general requirements relating to signage.



Avoid: Bright, reflective colours and multiple signage across a building facade.



Facade Treatment and Architectural Features

Objectives

- To ensure building façades are of a simple, modern architectural style and reflect bulky goods retailing, light industrial or commercial character, while taking into account the need to transition in scale and respond to the neighbouring residential area.
- To maintain and enhance the character of this precinct as a high quality, semi-industrial area.
- To ensure built form is of a quality that reflects the role precinct as an important entrance and gateway site in the north.
- To ensure new buildings positively contribute to the Kialla Lakes Drive streetscape.

Design Requirements

- Facade treatments should incorporate architectural features to the building frontage that address the street, provide legibility for customers, and reduce the visual impact of the large building mass.
- Provide clearly defined building entrances that are oriented towards the streetscape and have human scale elements.
- Enhance the relationship between built form and the streetscape by providing large window proportions, particularly on the front building elevation.
- Provide articulated building frontages that reduce the impact of visual bulk on the streetscape.
- Avoid large, blank walls or uniform front façades that do not provide for interaction with the streetscape and human scale.
- Buildings, works, plant and machinery should be constructed, housed and maintained in a manner that minimises visual impact.



Encourage: Muted colours and tonings, large window proportions oriented towards the streetscape and use of verandahs to minimise visual bulk

Materials, Colours and Finishes

Objectives

- To reflect the vision for Kialla Lakes Drive.
- To complement the colours and tones found in the nearby residential area.

Design Requirements

- New buildings should incorporate a material and colours palette reflective of the neighbouring residential area in muted/ natural colours and tonings such as the browns of brick and timber, natural creams and non-reflective finishes.
- Exterior walls should be of brick, concrete, steel or glass.
- Concrete tilt panel buildings should display a trowel or textured render finish in non-reflective, earthy tones to avoid large bland areas.
- Reflective glass, PVC, unrelieved painted render, highly reflective materials such as zincalume, unarticulated concrete surfaces and unarticulated cladding systems should be avoided. Bright, extravagant colours schemes should also to be avoided.
- Colour schemes of all external surfaces of new developments should consider neutral colours.
- Roof materials must be non-reflective to ensure no glare is projected within the Airport Environs area.



Avoid: Large, blank façades and use of bright, eye catching colours that are not sensitive to the residential area nearby



Encourage: Use of muted colours and tonings that draw on the agricultural and landscape settings that surround Shepparton

3.3 Summary of Findings

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KIALLA LAKES DRIVE ADDENDUM

Kialla Lakes Drive

Summary

The Kialla Lakes Drive subject area contains a transition in land uses from light industrial and commercial (IN3Z) in the west to residential (NRZ1) in the east. The stretch of Kialla Lakes Drive between GVH and the NRZ is highlighted as a gateway site where the sensitive interface requires improved management.

Further, most parcels within this subject area are currently vacant, and two areas of surplus land in the road reserve will soon be made available for development.

Recommendations

It is recommended that the Framework addendum contained within this chapter be used to support an expansion of the DDO7 to also apply to the subject area.

Where additions have been made to the objectives and recommendations unique to this new section of Kialla Lakes Drive, it is recommended that the DDO7 be amended to reflect those changes.

It is noted that the draft DDO originally prepared in the previous version of this report has been updated to reflect the requirements of the *Ministerial Direction on the Form and Content of Planning Schemes* (May 2017). The appended DDO schedule is now that updated version.

A

Appendix

Planning Practice Note 17: Urban Design
Frameworks

July 2015



APPENDICES

Planning Practice Note 17: Urban Design Frameworks

Overview

According to *Planning Practice Note 17 (PPN17): Urban Design Frameworks* (July 2015), an Urban Design Framework (UDF) should:

- include a comprehensive analysis of context
- look beyond the individual project and seek to coordinate across projects and opportunities, setting a project in its broader context
- incorporate and respond to information from existing strategies and studies, such as transport, heritage and neighbourhood character studies
- respond to all major stakeholders by integrating their interests and concerns
- incorporate major infrastructure issues and provide design direction for the details within infrastructure construction projects
- set out an implementation strategy that looks at a range of time scales and generally includes:
 - long-term strategies and options (say, 10 to 15 years and beyond)
 - intermediate-term strategies and options (say 3 to 5 years)
 - short-term actions that can be immediately

implemented without compromising long-term objectives

- present analysis, principles and options for implementation in graphical and written format to a standard suitable for public consultation
- illustrate existing constraints and available opportunities, identify different options, and provide a record of why particular options are selected referring to key policy objectives, urban design principles, etc.
- provide a 'layered' response to issues at hand, beginning with the broad contextual issues and principles, and working down to detailed design studies and guidelines for critical locations.

Urban Design Framework Process

The following steps should be undertaken when preparing a UDF (*PPN17*, 2015):

1. Preliminary actions (project objectives, scope, boundaries and significant influences in the region, including population projections)
2. Analysis and objectives (review existing plans, policies, infrastructure programs, built form

physical characteristics, land use and movement patterns, local strengths and weaknesses culminating in key issues and opportunities)

3. Synthesis and strategic framework (broad design objectives and strategic action areas, test options with stakeholders)
4. Final reporting, including an implementation strategy and coordinating framework plan.

The existing *Shepparton North & South Business Areas UDF* identifies and addresses five precincts in the Shepparton business areas, and sets out frameworks to address any threats and opportunities identified in the contextual analysis section of the report.

The framework sections of the UDF are focussed on structure, form and character of the identified precincts, and set out a number of strategies that can be used to achieve preferred character in new developments.

The following sections examine the report with respect to the two subject areas currently being examined as part of this project.



B

Appendix

Draft Design and Development Overlay

Shepparton North Gateway Commercial Precinct



APPENDICES

Draft Design and Development Overlay - Shepparton North Gateway Commercial Precinct

A draft DDO Schedule for the Shepparton North Gateway Commercial Precinct has been prepared and is included over the following pages.

It is noted that following release of the *Ministerial Direction on the Form and Content of Planning Schemes*, further revisions to the proposed DDO were made. The updated version is now included over the following pages.



APPENDICES

GREATER SHEPPARTON PLANNING SCHEME

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SCHEDULE 9 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO9**.

SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

1.0

Design objectives

To encourage a vibrant commercial area supporting large scale, regionally significant service and retail activities that reflect the strategic importance of the area as a gateway location in Shepparton's north.

To ensure the design, quality and siting of buildings reflects and enhances the role of the area while making efficient use of the land. This includes at prominent corners and sites at the northern and southern extents of the precinct, where innovative, high quality design is particularly important to strengthen the gateway role of the area.

To ensure signage is appropriately designed, sited and scaled for the gateway corridor context and minimises visual clutter.

To maintain a sense of openness and views to the surrounding landscape corridor from the Goulburn Valley Highway, and integrate public realm planting and a high quality public that appropriately within the surrounding landscape.

To implement the design and development guidelines for the the Goulburn Valley Highway in accordance with the *Urban Design Framework – Shepparton North & South Business Areas 2006* and the *Addendum to the Urban Design Farmework – Shepparton North & South Business Areas 2017*.

2.0

Buildings and works

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A permit is required to:

- Construct a fence that exceeds 1.2m in height above natural ground level, where the fence is located along a boundary that has a street frontage and is:
 - Visually impermeable; or
 - Constructed of unpainted or galvanised steel or uncoated wire.

Design Requirements

The following buildings and works requirements apply to an application to construct a building or to construct or carry out works, to the satisfaction of the responsible authority:

Building design

- The size of new buildings should be in keeping with the large format scale of surrounding development.
- Built form should be articulated and maximise glazing to avoid blank and dominant façades and create an appropriate transition between pedestrian entries, offices and warehouse areas.
- Offices or building areas open to the public should be located at the front of the building massing.
- Upper level building setbacks should be used to reduce building bulk and add visual interest.
- Street setbacks must incorporate significant landscaping and sealed car parking, and be utilised to provide clear sightlines and legibility to the street.
- Building roof form should be designed to complement the prevailing character.

GREATER SHEPPARTON PLANNING SCHEME

- Plant equipment, vents and any other mechanical equipment must be carefully designed and sited or incorporated into the roof design, to ensure it is screened or concealed from the street, surrounding public spaces and buildings.
- Where possible, ensure verandahs and canopies are incorporated into single storey built form components to enhance site amenity and provide shade for employees and visitors to the site.
- Sustainable water use in buildings should be provided by implementing measures to collect rain water runoff from roof areas, and ensure water storage/tanks are located away from public view, and do not impact on neighbours' visual amenity.
- Materials for external surfaces of new developments should consider and complement those used in existing buildings in the area, such as brick and non-reflective, high quality cladding.
- The use of concrete wall construction (e.g. tilt panels) should be complemented by the use of contrasting claddings such as timber and metals that provide facade definition.
- Reflective glass, PVC, unrelieved painted render, highly reflective materials such as zincalume, unarticulated concrete surfaces and unarticulated cladding systems should be avoided. Bright, extravagant colours schemes should also be avoided.
- Colour schemes of all external surfaces of new developments must complement those found in the surrounding area and should consider neutral colours.

Building height

- Buildings should be a maximum height of 11m above natural ground level, plus any applicable flood level determined by a relevant referral authority pursuant to this scheme. A building may exceed the maximum height by up to 1 metre if the slope of the natural ground level, measured at any cross section of the site of the building wider than 8 metres, is greater than 2.5 degrees. In that case, the building height should be 10 metres plus any applicable flood level determined by a relevant referral authority pursuant to this scheme. See Diagram 3.
- The street wall height of a building frontage should not exceed two storeys (or 6.9m) above natural ground level. See Diagram 3.

Building setbacks

- Building setback should be a minimum of 9m and maximum of 20m from the front lot boundary, to the satisfaction of the responsible authority. See Diagram 4.
- Where a zero (0m) setback is sought on land adjacent to the GRZ1, the provisions of Clause 55 must be considered with regard to overshadowing and overlooking.
- Where possible, development should be set back from at least one side boundary by a minimum of 1m to enable views from the Goulburn Valley Highway between buildings to the surrounding area.
- Loading and unloading areas should be set back a minimum distance of 20m from the front lot boundary.

Fences

- Fencing should be constructed from materials that complement the building and surrounding area and should be painted in muted colours/tonings, avoiding galvanised steel mesh and 'Colorbond' style fencing.
- Front fences should have a minimum 50% visual permeability.
- Where possible, fencing should be softened and screened by vegetation planting.

Parking and access

- Sufficient car parking spaces should be provided on site for occupants and visitors in accordance with Clause 52.06 of the Greater Shepparton Planning Scheme.

GREATER SHEPPARTON PLANNING SCHEME

- For larger sites, consideration should be given to short-term parking areas located in the front setback, with longer-term parking (i.e. staff parking) located to the rear.
- Avoid large paved car parking areas that are not broken up by landscaping.
- Ensure a landscape buffer is provided between car parking areas and the front property boundary.
- Where practicable, use landscape design to provide significant canopy trees that can assist in shading car parking areas to mitigate heat impacts.
- Where practicable, limit vehicle entry points to one crossover and avoid large areas of circular driveway aprons and impermeable surfaces.
- Group access points and limit double crossover widths to large sites to minimise opportunities for vehicle conflict between the service road and GVH.
- For corner sites, encourage vehicle access to a side road rather than the GVH service road.
- For the purpose of providing an active street frontage, not more than 7.5m on any site frontage should be utilised for vehicular access purposes. See Diagram 6.

Landscaping

- A minimum 1.5m landscape buffer must be established along rear boundaries to ensure suitable interface with adjoining residential lots in the GRZ1. See Diagram 5.
- A minimum 1.5m landscape buffer should be established to ensure suitable interface with all other adjoining zones.
- A minimum of 15% of the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree (mature height 10m). Plant species must be appropriate and suitable to this area, and to Council's satisfaction.
- Provide large canopy trees and understorey planting to minimise loss of views from the GVH corridor to the surrounding landscape.
- Minimise areas of paving, particularly in the front setback.

Advertising signage

- Panel, Promotion and Major promotion signs will only be considered acceptable on a temporary basis (e.g. a maximum of 15 years from the date of permit issue)
- Free-standing signage (e.g. pole signs) must be set back a minimum distance of 1m from the front property boundary.
- Signage, including painted signs, located on a side wall should not occupy more than 50% of the area of the wall.
- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2m and maximum width of 1.5m. This envelope includes the height of any supporting structure. See Diagram 1.
- Signs attached to a building are encouraged and should be a maximum height of 1m and a maximum width of 3m. Exemptions may be made for signs composed of individual letters that form an integral part of the building facade. See Diagram 2.

Application Requirements

An application for buildings and works must be accompanied by the following information, to the satisfaction of the responsible authority, as relevant:

- A written response detailing how the proposal appropriately responds to the objectives and provisions of the Schedule.
- A detailed landscape plan and irrigation system demonstrating how the development will implement water wise, water sensitive urban design and low water use plan materials, to the satisfaction of the responsible authority.

GREATER SHEPPARTON PLANNING SCHEME

An application to vary the requirements set out in a Sub-clause to this schedule must provide a written submission demonstrating how the proposal responds to the strategic and gateway significance of the corridor, while providing:

- Innovative, high quality design;
- Efficient use of the land; and
- Enhancement of the appearance and strengthening the identity of the commercial corridor.

This requirement applies to proposals on prominent sites such as corners and sites at the northern and southern extents of the precinct.

Exemption from notice and review

An application for construction of a building or to construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

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Subdivision

None specified.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0

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Advertising signs

Advertising sign requirements are at Clause 52.05. All land located within the Shepparton North Gateway Commercial Area Precinct is in Category 1 – Commercial Areas.

In addition to the relevant requirements at Clause 52.05, an application for advertising signage should meet the following requirements, to the satisfaction of the responsible authority:

- Business identification signage should form an integral part of the front building façade, be appropriately proportioned to sit within the parapet or gable end, and not protrude above or beyond the building façade.
- Business identification information can be incorporated into one way-finding oriented sign at the entrance to each section of service road to reduce visual clutter.
- Major promotion signs should be attached to and integrated into the front building facade.
- High wall and sky signs are discouraged.
- Panel and Promotion signs that project outside the perimeter of the building, or are free-standing, are discouraged.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are electronic, animated, contain moving parts or have flashing elements are not encouraged.
- Bunting, animated and reflective signs, including sandwich boards and inflatable and temporary signs, are discouraged.

GREATER SHEPPARTON PLANNING SCHEME

- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.

5.0

Decision guidelines

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Proposed
C196

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The design and development guidelines set out in the *Addendum to the Urban Design Framework - Shepparton North & South Business Areas* (Planisphere, 2017).

Building design and siting

- Whether the height of a proposed building accords with the scale of the local environment and type of surrounding buildings.
- The design and siting of existing development in the area.
- The interface with development on adjoining zones outside the Commercial 1 Zone (C1Z) or Commercial 2 Zone (C2Z).
- Whether the proposed development appropriately responds to and enhances the character of the *Shepparton North Gateway Commercial Precinct* and its gateways.
- Whether the proposed front setbacks enable efficient use of the land.
- Whether opportunities for views between buildings to the surrounding area are available.
- Whether there is any potential for land use conflicts with adjoining zones as a result of the proposed building siting.
- Whether the development has been designed in a manner that is climatically appropriate for the local context.
- The potential for the proposed building to impact visual amenity of surrounding lots.
- Whether the proposed building incorporates active and attractive elements that interact with the streetscape.
- Whether, in seeking to vary any requirements in this Schedule, the proposed development:
 - Achieves innovative, high quality design and efficient use of the land; and
 - Enhances the appearance and strengthens the identity of the commercial corridor.

Landscaping

- Whether adequate space has been provided for landscaping within the front setback.
- Whether plant choice is sustainable and suitable within the local environment.
- Where front fencing is proposed, whether the proposed fence enhances the street appearance and maintains view to, between, and around sites.
- Whether the proposed landscape concept plan responds to Council's *Landscape Plan Guide* (Spiire, 2017) and any relevant planting schedule.

Advertising signage

GREATER SHEPPARTON PLANNING SCHEME

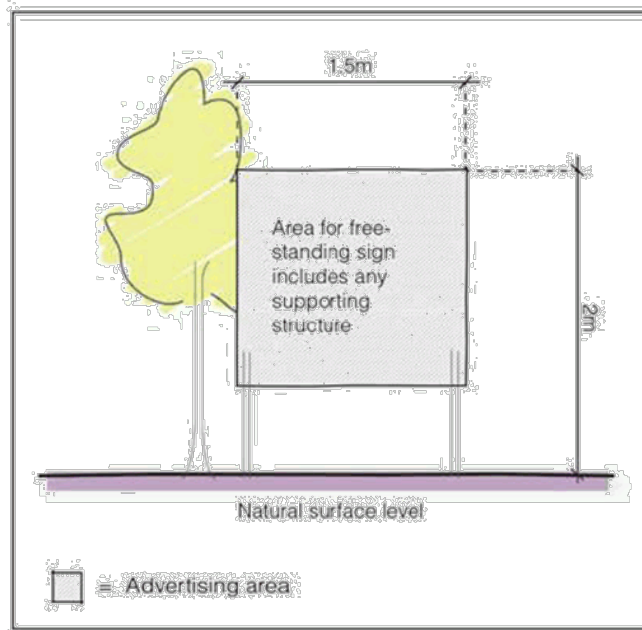
- Whether the proposal is appropriately designed, sited and scaled for the Goulburn Valley Highway corridor without detracting from the public realm or competing with way-finding and road signage.
- Whether colours and materials proposed might interfere with the safety or efficiency of the Goulburn Valley Highway as a Road Zone, Category 1 (RDZ1).



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GREATER SHEPPARTON PLANNING SCHEME

Diagram 1: Freestanding business identification sign envelope



APPENDICES

Diagram 2: Signs attached to buildings

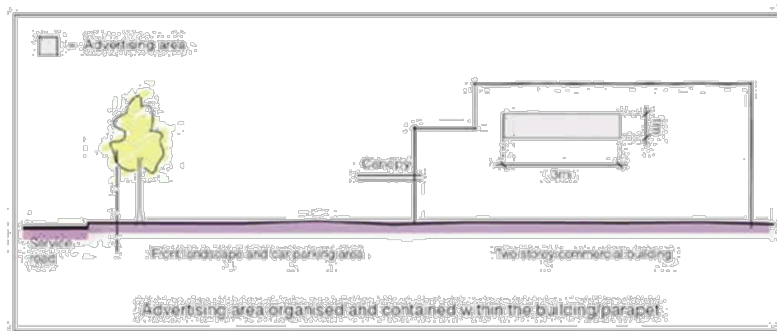
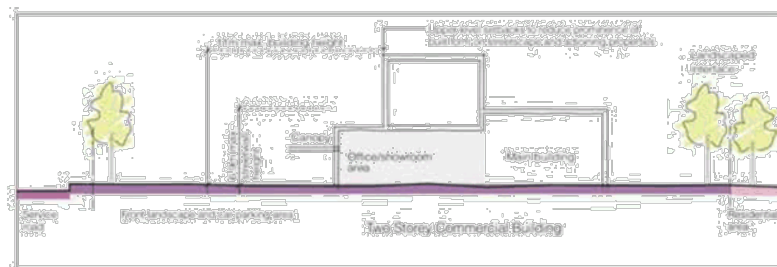
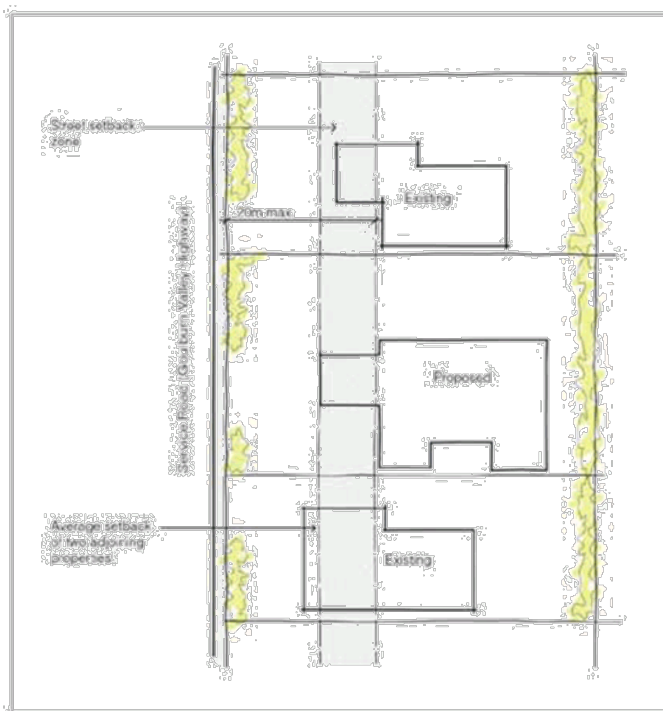


Diagram 3: Building heights and setbacks



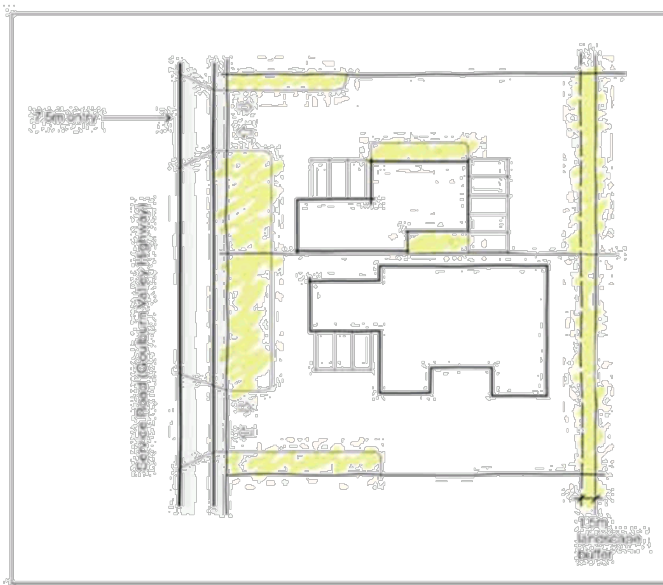
GREATER SHEPPARTON PLANNING SCHEME

Diagram 4: Site layout and design



APPENDICES

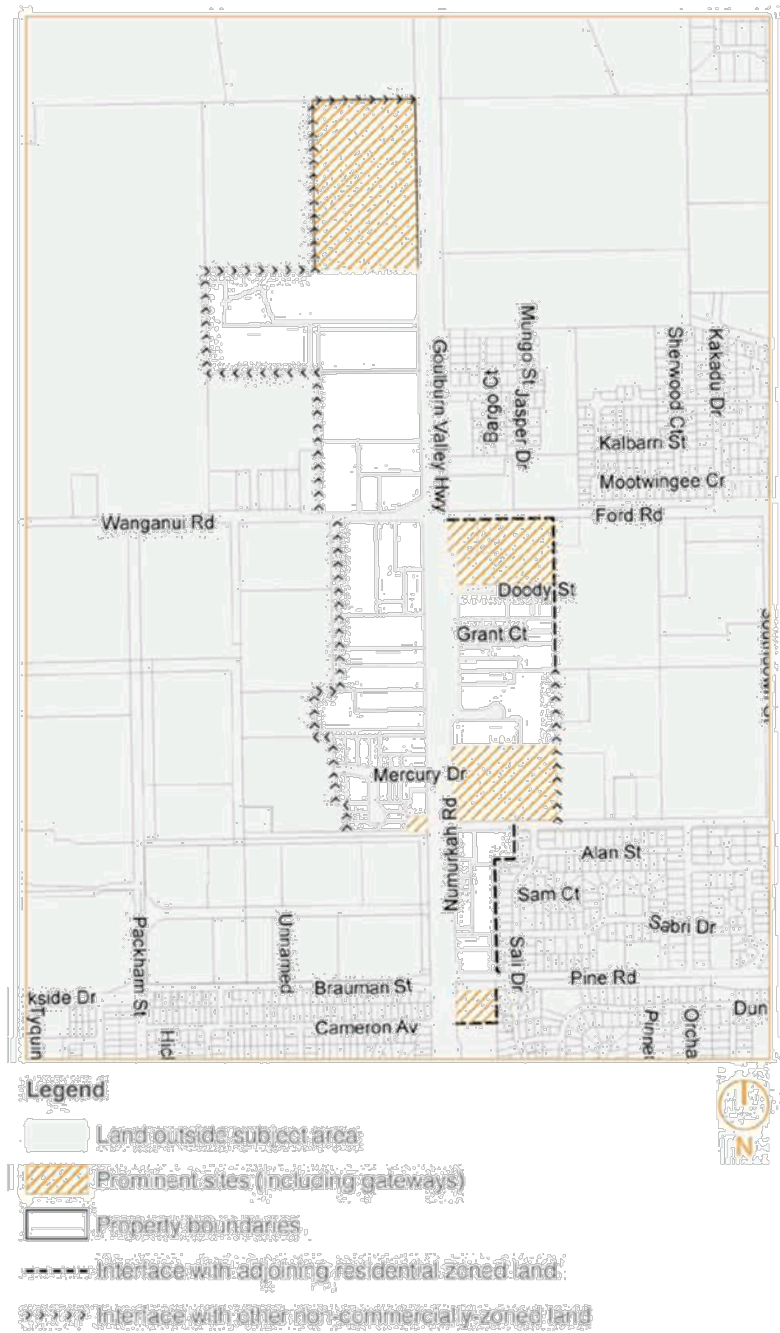
Diagram 5: Landscape buffer



GREATER SHEPPARTON PLANNING SCHEME

Map 1: Framework Plan

APPENDICES



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Appendix

Draft Design and Development Overlay 7

Kialla Park Boulevard Precinct



APPENDICES

Proposed Amendments to DDO7 (Kialla Park Boulevard Precinct)

The proposed amendments to the existing Design and Development Overlay that applies to the Kialla Park Boulevard Precinct (DDO7) are included as tracked changes over the following pages.

The amendment includes extending the current area of DDO7 to include the Kialla Lakes Drive subject area investigated as part of the current study. The proposed changes include design requirements and considerations that apply to all land currently covered by the DDO7 and include additional lots as shown on the Kialla Park Boulevard Framework Plan on page 43 and included as 'Map 1' on the DDO7 set out below; this includes the former drive-in site (Riverside Plaza).

It is noted that following release of the *Ministerial Direction 13 on the Form and Content of Planning Schemes*, further revisions to the DDO7 were made. The updated version is now included over the following pages.



APPENDICES

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SCHEDULE 7 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO7**.

KIALLA PARK BOULEVARD PRECINCT

1.0

Design objectives

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To create a vibrant and active commercial and business precinct based around large buildings for bulky goods retailing, manufacturing and associated business services within a well-landscaped boulevard setting.

To encourage high quality built form that strengthens and enhances the existing ‘big box’ commercial character of the area, including the provision of front setbacks that can accommodate high quality landscape treatments and the display of goods.

To ensure landscape treatments and signage support and enhance the Goulburn Valley Highway as a key gateway to Shepparton, complement the existing Kialla Lakes Drive and Goulburn Valley Highway landscapes, and reinforce landscaped street edges.

To ensure new development provides clearly defined, legible and attractive gateways to residential areas, particularly at Kialla Lakes Drive, and an appropriate built form and landscape interface between commercial and residential areas.

To implement the design and development guidelines for the Kialla Park Boulevard along the Goulburn Valley Highway and Kialla Lakes Drive in accordance with the *Urban Design Framework – Shepparton North & South Business Areas 2006* and the *Addendum to the Urban Design Framework – Shepparton North & South Business Areas 2017*.

2.0

Buildings and works

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Permit requirements

A permit is required to:

- Construct a fence that exceeds 1.2m in height above natural ground level, where the fence is located along a boundary that has a street frontage, and has a transparency ratio of less than 50%, including piers, columns and bases.

Design Requirements

The following buildings and works requirements apply to an application to construct a building or to construct or carry out works, to the satisfaction of the responsible authority:

Building height

- Buildings should be a maximum height above natural ground level as specified in the table below.
- The street wall height of a building facade should not exceed the height above natural ground level specified in the table below.

<i>Precinct (shown on Map 1)</i>	<i>Building height</i>	<i>Street wall height</i>
Goulburn Valley Highway Precinct	14.5m maximum	8m or three storeys
Kialla Lakes Drive Precinct	11m maximum*	6.9m or two storeys*

GREATER SHEPPARTON PLANNING SCHEME

Building setbacks

- Building setback should be 20 metres from the front boundary.

A lesser setback (not less than 9 metres) may be considered by the responsible authority for the properties listed below for a single dwelling in a General Residential Zone, Neighbourhood Residential Zone or a Residential Growth Zone.

- No. 2 Reserve Street Kialla (Lot No. 1 PS327036F Parish of Kialla)
- No. 4 Reserve Street Kialla (Lot No. 2 PS327036F Parish of Kialla)
- Nos 1/6 and 2/6 Reserve Street Kialla (Lot No. 3 LP63440 Parish of Kialla)
- No. 10 Reserve Street Kialla (Lot Nos. 4 and 5 LP63440 Parish of Kialla)
- No. 12 Reserve Street Kialla (Lot No. 6 LP63440 Parish of Kialla)
- No. 14 Reserve Street Kialla (Lot No. 7 LP63440 Parish of Kialla)
- No. 16 Reserve Street Kialla (Lot No. 8 LP63440 Parish of Kialla).

A lesser setback (not less than 9 metres) may be considered by the responsible authority for the properties listed below for buildings and works in a Commercial Zone or Industrial Zone.

- Lot 1 and Lot 2 PS433159.
- Lot 1, Lot 2 and Lot 3 PS439192.
- Lot 2 PS426100.
- Lot 1 and Lot 2 LP121036.

Parking and access

- Encourage vehicular access points and driveways to be designed to allow convenient, safe and efficient vehicle movements and connections within the development and to the street network. All vehicles including those delivering to or servicing the site should be able to enter and exit the site in a forward direction.
- A vehicular access point across lot frontage is to be no more than 6.4 metres wide
- Driveways should be designed to minimise any conflict of vehicle movements with pedestrians including consideration of side fences and / or buildings that will impact on sight lines between pedestrians and vehicular traffic.
- Adequate on site parking in accordance with the Greater Shepparton Planning Scheme should be provided in a form and manner that will not reduce the amenity of the area.
- Encourage developers to consider planting trees throughout car parking areas.

Landscaping

- A minimum 3 metre landscape buffer must be provided along rear boundaries to ensure suitable interface to residential properties. Where a residential zone abuts a side boundary, a minimum 1.5m landscape buffer must be provided.
- A minimum of 15% of the area to the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree with a mature height of 10 metres (except where the land is also affected by DDO2, when the maximum height should not exceed 7.5metres, except with a Permit issued pursuant to that Schedule). All plant species must be suitable to this area and to the Council's satisfaction.

GREATER SHEPPARTON PLANNING SCHEME

- The front building setback should be landscaped to include a variety of shrubs and ground covers. Plant species should be suitable to this area, reflect streetscape planting schemes and be to the satisfaction of the responsible authority.
- Paving materials that provide texture, patterns, subtle colour and permeability to the lot frontage should be used. Large expanses of solid paving such as grey cement or asphalt should be avoided.
- Incorporate small to medium sized trees wherever possible to provide scale, aesthetic relief and shade to front entrances.
- Environmental weeds and invasive tree species should be avoided to the satisfaction of the responsible authority.

Application Requirements

An application for buildings and works must be accompanied by the following information, to the satisfaction of the responsible authority, as relevant:

- Applications within the Sub-Regional Centre (Riverine Plaza) should demonstrate how the proposal will support the site's ongoing function as a key component of the Kialla Lakes Boulevard Precinct.
- A landscaping plan together with proposed irrigation system should be submitted with applications for buildings and works to the satisfaction of the responsible authority.
- The landscape plan and irrigation system should implement water wise, water sensitive urban design and low water use plant materials to the satisfaction of the responsible authority.

Exemption from notice and review

An application for construction of a building or to construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

3.0

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Subdivision

None specified.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0

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Advertising signs

Advertising sign requirements are at Clause 52.05. All land located within the Kialla Park Boulevard Precinct is in Category 1 – Commercial Areas.

In addition to the relevant requirements at Clause 52.05, an application for advertising signage should meet the following requirements, to the satisfaction of the responsible authority:

- One business identification sign is permitted per development.
- Multiple business occupancies are to share space on the sign.

GREATER SHEPPARTON PLANNING SCHEME

- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2 metres, and a maximum width of 1.5 metres. This envelope includes the height of any supporting structure.
- Signs attached to a building are encouraged and should be a maximum height of 1.0 metre, and a maximum width of 3.0 metres. Exemptions may be made for signs composed of individual letters that form an integral part of the building façade.
- Panel, Promotion and Major Promotion signs should be avoided.
- Above verandah signs including V-board signs and advertising elements such as banners, flags and inflatable should be avoided.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are electronic, animated, contain moving parts or have flashing elements are not encouraged.
- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.
- Refer to Clause 52.05 of the Shepparton Planning Scheme for general requirements relating to signage.

5.0

Decision guidelines

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The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the proposal provides for a vibrant and active commercial development that reinforce the character of the area with consistent front setbacks for the display of goods in a well landscaped boulevard setting.
- Whether the height of a proposed building accords with the scale of the local environment and the type of surrounding buildings.
- The effect of the development of proposed buildings on the amenity of abutting buildings.
- Whether the proposed landscape treatment contributes to the character of Kialla Lakes Drive or the Goulburn Valley Highway landscape gateways.
- Whether the design, siting and appearance of buildings improves visual presentation of the frontages at the town entrance.
- The architectural quality and innovative response of the building design.
- Whether building setbacks provided along Kialla Lakes Drive or Goulburn Valley Highway demonstrate appropriate consideration of the streetscape and the residential interface.
- Whether the layout allows for safe access and egress from the site.
- The location of any proposed car parking.
- The inclusion of design elements which protect the amenity of abutting residents.
- Whether the development provides for a robust, suitably scaled landscape treatment that integrates the built form, provides an appropriate scale, reduces its

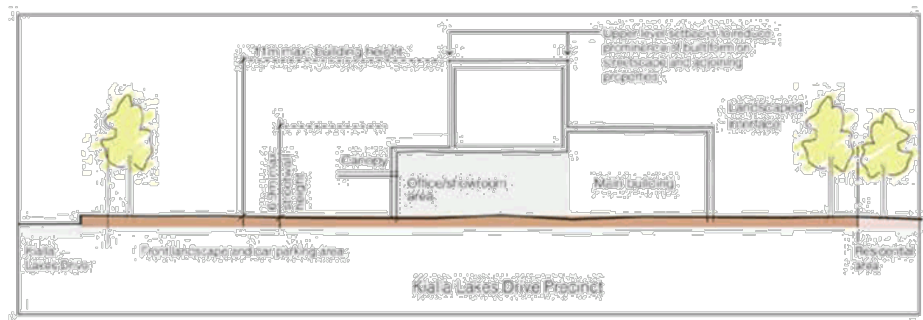
GREATER SHEPPARTON PLANNING SCHEME

visual impact and provides a suitable interface between the commercial and the residential areas.

- Whether the development provides landscape treatment that visually integrates the commercial areas and contributes to a pleasing streetscape, while also partially screening the buildings when viewed from the road.
- Whether the proposed development contributes to the significance of the Sub-Regional Centre providing specialty services to surrounding residential and business areas.
- Whether the proposed development on land abutting a residential zone complies with the overlooking and overshadowing provisions set out at Clause 54 or Clause 55, as appropriate.
- Whether the design considered energy and resource efficient and sustainable design principles.
- Whether the proposal is in accordance with the design and development guidelines set out in the *Urban Design Framework - Shepparton North & South Business Areas*, July 2006 and the *Addendum to the Urban Design Framework* (Planisphere, 2017).
- Whether the proposal complies with the provisions of DDO2 of this planning scheme to the satisfaction of the Responsible Authority.

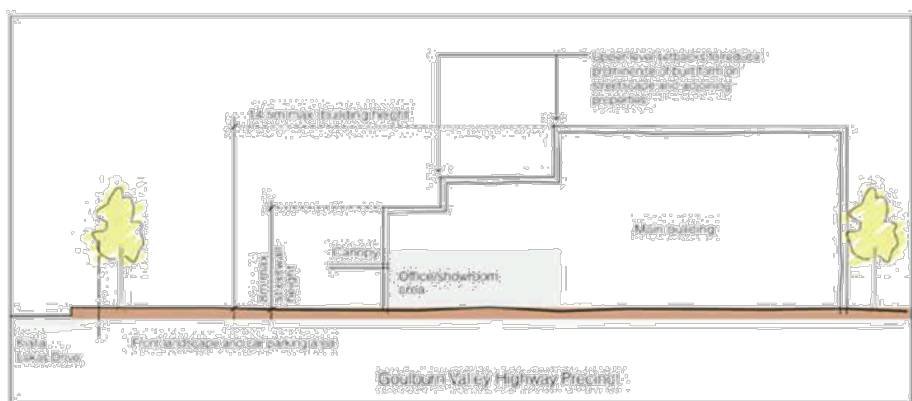
GREATER SHEPPARTON PLANNING SCHEME

DIAGRAM 1: BUILDING HEIGHT AND SETBACK IN KIALLA LAKES DRIVE PRECINCT



APPENDICES

DIAGRAM 2: BUILDING HEIGHT AND SETBACK IN GOULBURN VALLEY HIGHWAY PRECINCT



GREATER SHEPPARTON PLANNING SCHEME

DIAGRAM 3: FREESTANDING BUSINESS IDENTIFICATION SIGN ENVELOPE

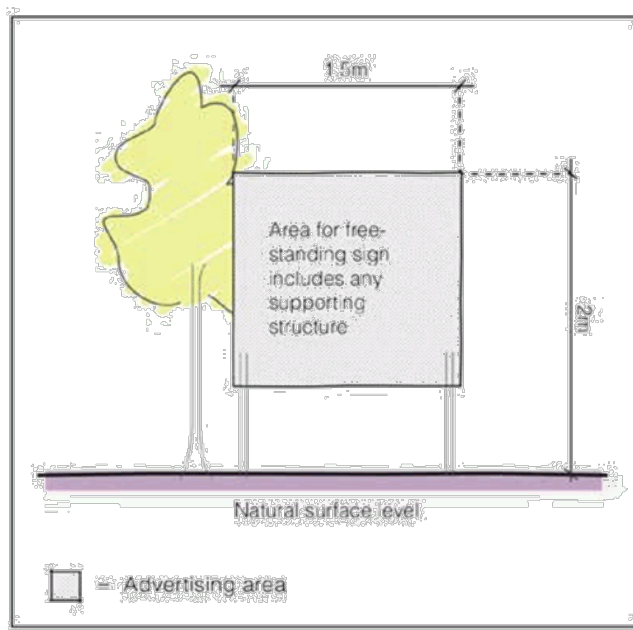
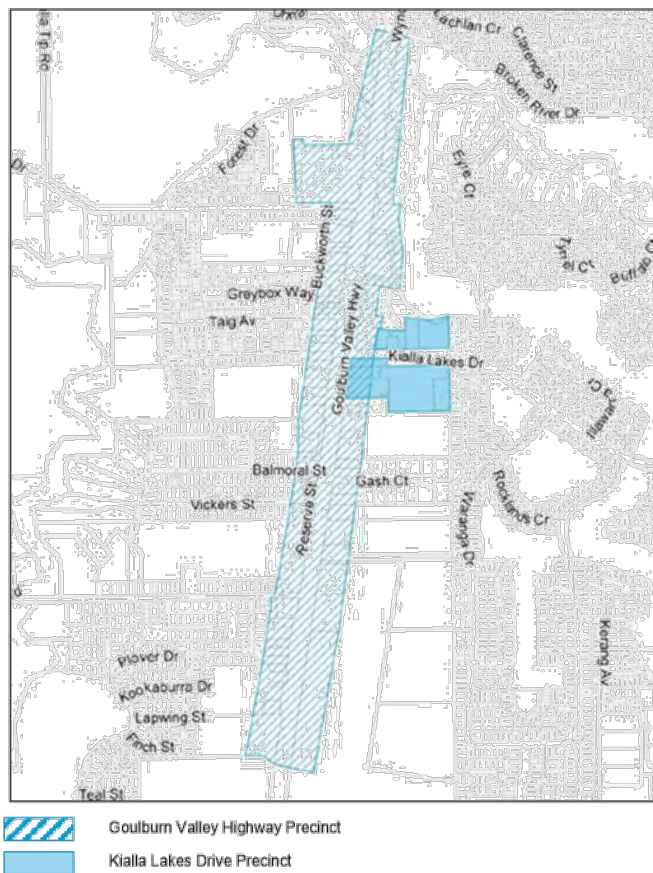


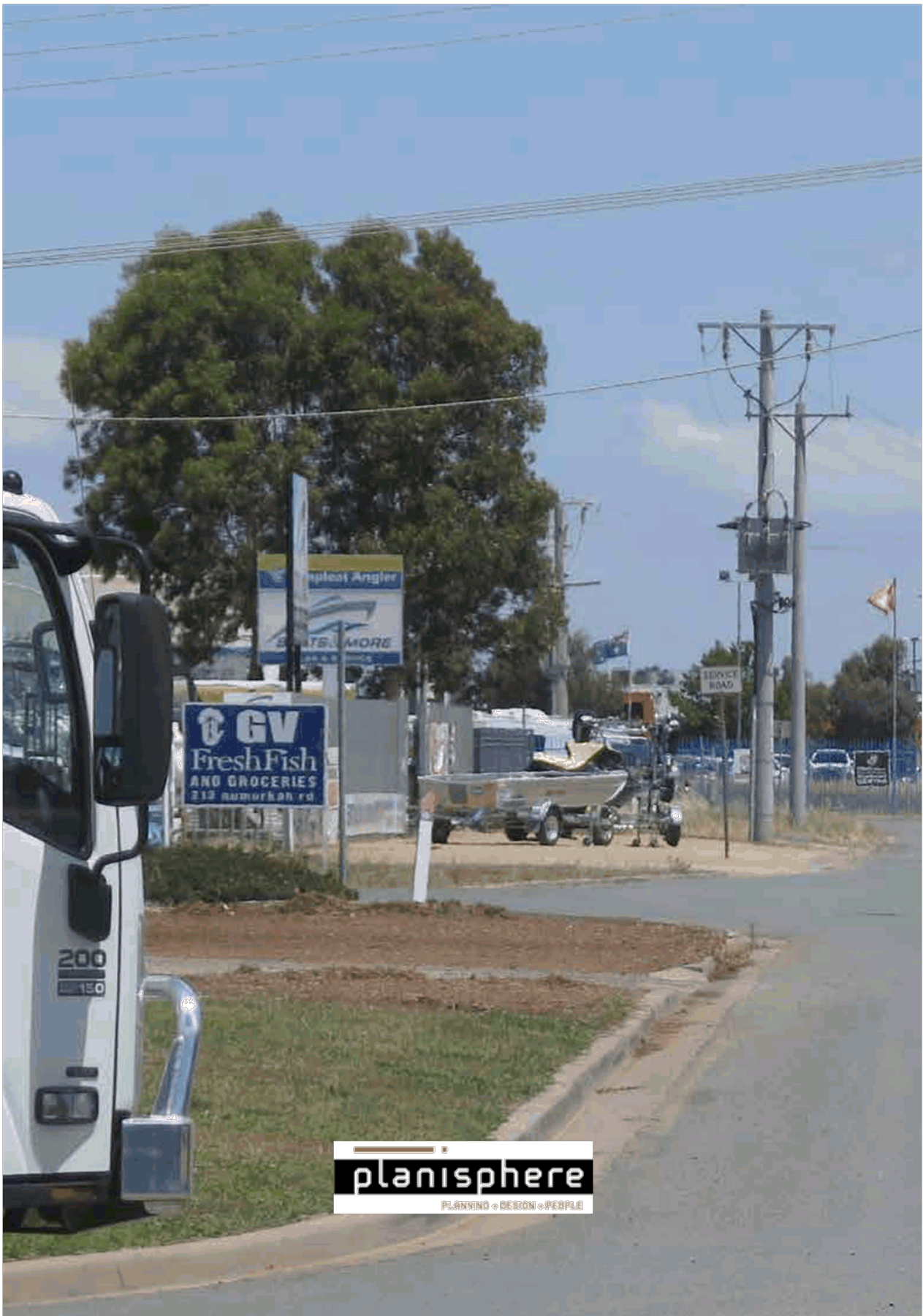
DIAGRAM 4: SIGNS ATTACHED TO BUILDINGS



GREATER SHEPPARTON PLANNING SCHEME

MAP 1: PRECINCTS





Planning and Environment Act 1987

Panel Report

Greater Shepparton Planning Scheme Amendment C196
Addendum to Urban Design Framework

16 February 2018



Planning and Environment Act 1987

Panel Report pursuant to section 25 of the Act

Greater Shepparton Planning Scheme Amendment C196

Addendum to Urban Design Framework

16 February 2018



Michael Ballock, Chair



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List of Abbreviations

C1Z	Commercial 1 Zone
C2Z	Commercial 2 Zone
DDO	Design and Development Overlay
DDO7	Design and Development Overlay Schedule 7
DDO9	Design and Development Overlay Schedule 9
MSS	Municipal Strategic Statement
UDF Addendum	Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017

Overview

Amendment summary	
The Amendment	Greater Shepparton Planning Scheme Amendment C196
Common name	Addendum to Urban Design Framework
Brief description	The Amendment proposes to introduce the Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017 to the Planning Scheme as a reference document and implement its findings to provide guidance for the design and development of land in Kialla and Shepparton North. The Amendment ensures that the existing DDO7 content aligns with the revised Ministerial Direction on the Form and Content of Planning Schemes.
Planning Authority	Greater Shepparton City Council
Authorisation	A03600 12 July 2017
Exhibition	From 3 August to 4 September 2017
Submissions	Number of Submissions: 12 Opposed: 7 (see Appendix A)
Panel process	
The Panel	Michael Ballock (replaced Brett Davis)
Directions Hearing	A Directions Hearing was first held on 4 October before the C192 and C193 Panel Report had been released. The Panel agreed to hold over the Hearing date to allow time for the report to be released, and parties to prepare. A second Directions Hearing was held at Council offices on 8 December 2017.
Panel Hearing	Greater Shepparton City Offices, Shepparton on 18 January 2018
Site inspections	Unaccompanied, 18 January 2018
Appearances	See Table 1
Date of this Report	16 February 2018

Executive summary

(i) Summary

Greater Shepparton Planning Scheme Amendment C196 (the Amendment) proposes to introduce the Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017 to the Planning Scheme as a reference document and implement its findings to provide guidance for the design and development of land in Kialla and Shepparton North by amending the existing Design and Development Overlay Schedule 7 and the introduction of a Design and Development Overlay Schedule 9.

The Amendment applies to three areas being:

- Kialla Park Boulevard Precinct
- Kialla Lakes Drive Entrance
- Shepparton North Commercial Gateway Precinct.

In addition, the Amendment ensures that the existing Design and Development Overlay Schedule 7 content aligns with the new Ministerial Direction on the Form and Content of Planning Schemes.

The overlays are intended to improve the entrances to the town by providing built form guidance for development along and adjoining the Goulburn Valley Highway.

The key issues raised in submissions focused on the setback and landscaping requirements contained in the overlays.

Council has revised the Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017 and both overlays in response to submissions and to provide flexibility for gateway sites to achieve good urban design outcomes.

The Panel concludes that the Urban Design Framework: Shepparton North and South Business Areas July 2017 is well founded and an appropriate strategic basis for the controls proposed in the overlays.

(ii) Recommendations

Based on the reasons set out in this Report, the Panel recommends that Greater Shepparton Planning Scheme Amendment C196 be adopted as exhibited subject to the following:

- Replace the exhibited maps 13DDO and 14DDO with the revised versions provided in Appendix C.**
- Replace the exhibited Design and Development Overlay Schedule 7 with the Panel preferred version provided in Appendix D.**
- Replace the exhibited Design and Development Overlay Schedule 9 with the Panel preferred version provided in Appendix E.**

1 Introduction

1.1 The Amendment

(i) Amendment description

Specifically, the Amendment proposes to:

- amend Clause 21 09 (Reference Documents) to include the Urban Design Framework Shepparton North and South Business Areas July 2017 (UDF Addendum) as a reference document
- amend DDO7 to revise the design and development guidance and align with the Ministerial Direction on the Form and Content of Planning Schemes
- apply the Design and Development Overlay Schedule 7 (DDO7) to land at the Kialla Lakes Drive Entrance
- insert a new Design and Development Overlay Schedule 9 (DDO9) to implement the recommendations of the UDF Addendum for the Shepparton North Gateway
- apply the DDO9 to land in Shepparton North
- insert new Planning Scheme Map Nos 13DDO and 14DDO
- amend Planning Scheme Map No 26DDO
- amend the Schedule to Clause 61.03 - *What does this Scheme consist of?*.

(ii) Purpose of the Amendment

The purpose of the Amendment is to introduce the UDF Addendum to the Planning Scheme as a reference document and implement its findings to provide guidance for the design and development of land in Kialla and Shepparton North.

(iii) The subject land

The Amendment applies to the following three precincts:

- The Kialla Park Boulevard Precinct, highlighted in purple in Figure 1, and generally includes land on both sides of the Goulburn Valley Highway between Broken River Drive at the northern extent and the Shepparton Aerodrome at the southern extent. The DDO7 already applies to this land.
- The Kialla Lakes Drive Entrance, highlighted in blue in Figure 2 generally includes land on both sides of the entrance to Kialla Lakes Drive from the Goulburn Valley Highway. The Amendment proposes to apply the DDO7 to this land
- The Shepparton North Commercial Gateway Precinct, highlighted in blue in Figure 3, and generally includes land on both sides of the Goulburn Valley Highway (Numurkah Road) from immediately south of the Pine Road intersection at the southern extent, to the commercial land north of the Wanganui Road intersection at the northern extent. The Amendment proposes to apply the DDO9 to this land.

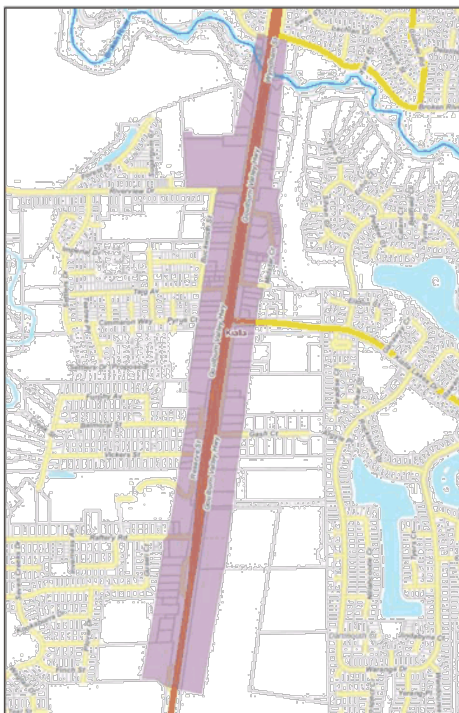


Figure 1: Kialla Park Boulevard Precinct

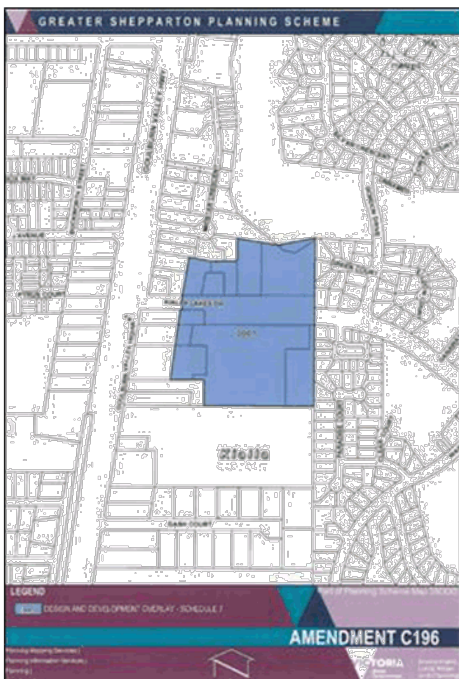


Figure 2: Kialla Lakes Drive Entrance

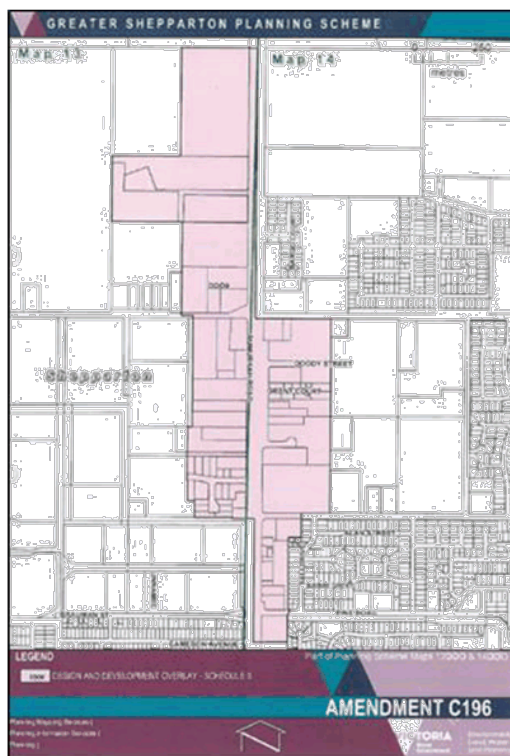


Figure 3: Shepparton North Commercial Gateway Precinct

1.2 Panel process

The Amendment was prepared by the Greater Shepparton City Council as Planning Authority. As exhibited, the Amendment proposes to:

- include the UDF Addendum as a reference document
- revise the DDO7
- insert a new DDO9 and apply it to the Shepparton North Gateway
- amend the planning scheme maps and Clause 61.03.

The Amendment was authorised by the Department of Environment, Land, Water and Planning, under delegation from the Minister for Planning, on 12 July 2017.

The Amendment was placed on public exhibition between 3 August and 4 September 2017, with 7 opposing submissions received.

Directions Hearings were held in relation to the Amendment on 4 October and 8 December 2017.

The Panel then met in the offices of Greater Shepparton Council on 18 January 2018 to hear submissions about the Amendment. Those in attendance at the Panel Hearing are listed in Table 1.

Table 1 Parties to the Panel Hearing

Submitter	Represented by
Greater Shepparton City Council	Mr John Keaney of Keaney Planning Pty Ltd
Alan and Michele Smith	
Leo Schoonderbeek	
Andrew Cleave	

1.3 Procedural issues

The Panel, comprising Brett Davis, was appointed under delegation from the Minister for Planning on 8 September 2017 under the provisions of section 153 of the *Planning and Environment Act 1987* to consider submissions lodged in respect of Amendment C196. At the Directions Hearing held on 4 October 2017, several parties requested that the Amendment be deferred pending the release of the Greater Shepparton Amendments C192 and C193 Panel Report. It was submitted that this report had ramifications for the Commercial 1 zoned land owned and operated by both Marl Enterprises Pty Ltd and Shepparton Pty Ltd.

On the 6 November 2017, the Panel Report for Amendments C192 and C193 was made public. On 21 November 2017, Council considered the recommendations of the C192 and C193 Panel Report and resolved to adopt part of the recommendations of the Panel.

On 23 November 2017, the first of several letters was received from Wisewould Mahony on behalf of Marl Enterprises Pty Ltd requesting that the Hearing set for 8 December 2017 be adjourned for various reasons including:

- *The Council resolution of 21 November does not follow the Panel recommendations and at that time the minutes were not yet made public*
- *An application for review has been filed with VCAT for Stage 2 of the Shepparton North Shopping Centre*
- *Seeking an adjournment until at least April 2018, citing natural justice or significant prejudice reasons.*

On the same day, Minter Ellison wrote to the Panel on behalf of Shepparton Pty Ltd, (formerly 18 Pty Ltd), requesting an adjournment of the Hearing until the VCAT outcome is known. It also noted, amongst other things that:

- *Amendment C196 is premature*
- *There is a need for a comprehensive structure planning process for this area, as intimated by the C192/C193 Panel*
- *Sufficient time is required for their client to allow review of that report and consider the recommendations*

On 27 November 2017, the Panel determined that the Hearing would not be held on 8 December 2017.

On 8 December 2017, a second Directions Hearing was held. In response to submissions Council advised the Panel:

Based on discussions held at the Directions Hearing, Council proposes an additional post-exhibition change to Amendment C196 to remove the proposed Schedule 9 to the Design and Development Overlay (DDO9) from land within the Commercial 1 Zone (see attached marked-up draft DDO9 map). The only land within the Commercial 1 Zone in the proposed DDO9 is the land at 177-193 Numurkah Road, Shepparton, known as the "IGA site". The application of the proposed DDO9 for all other land within the corridor would remain unchanged.

Subsequently on 13 December 2017 both Marl Enterprises Pty Ltd and Shepparton Pty Ltd withdrew their requests to be heard. The post-exhibition mapping change is shown at Appendix C.

Marl Enterprises Pty Ltd submitted it still requested the Panel to take into account its written submissions and have regard to Amendments C192 and C193 and the need to define Shepparton North and prepare a structure plan. This is discussed in more detail in Section 3.

The Hearing was reconvened on 18 January 2018. During this time, the Chair was reconstituted to Michael Ballock who was appointed on 2 January 2018 under the provisions of section 153 of the *Planning and Environment Act 1987*.

Leo Schoonderbeek informed the Panel that he had not been advised of the Hearing and consequently had not submitted a request to be heard from. The Panel directed that Mr Schoonderbeek could make a submission to the Panel.

1.4 Background to the proposal

In December 2016, Council reviewed the 2006 Urban Design Framework (2006 UDF). This review included:

- a review of the above relevant VCAT decisions
- contextual analysis of the commercial land in Shepparton North
- the development of a framework for the commercial land in Shepparton North
- a new schedule to the Design and Development Overlay (DDO) for the Shepparton North Gateway.

At its meeting held on 18 April 2017, Council resolved to endorse the draft UDF Addendum and release it for public comment from 24 April to 19 May 2017. All land owners and occupiers were notified about the document. A total of five submissions, mostly in support, were received on the draft UDF Addendum.

Council revised the UDF Addendum to address the concerns raised in these submissions. In addition, the draft DDO provisions were revised to provide flexibility for gateway sites to achieve good urban design outcomes. The documentation was also revised to align with the Ministerial Direction on the Form and Content of Planning Schemes.

1.5 Issues dealt with in this Report

The Panel considered all written submissions made in response to the exhibition of the Amendment, observations from site visits, and submissions and other material presented to it during the Hearing.

The Panel has had to be selective in referring to the more relevant or determinative material in the Report. All submissions and materials have been considered by the Panel in reaching its conclusions, regardless of whether they are specifically mentioned in the Report.

This Report deals with the issues under the following headings:

- Planning context
- Design and Development Overlay Schedules 7 and 9.

2 Planning context

Council provided a response to the Strategic Assessment Guidelines as part of the Explanatory Report.

The Panel has reviewed Council's response and the policy context of the Amendment and has made a brief appraisal of the relevant zone and overlay controls and other relevant planning strategies.

2.1 Policy framework

(i) State Planning Policy Framework

Council submitted that the Amendment is supported by the following clauses in the State Planning Policy Framework:

- Clause 15.01-2 (Urban design principles) contains the following strategy *"to achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm while minimising detrimental impact on neighbouring properties."*
- Clause 15.01-2 (Landscape architecture) states that *"recognition should be given to the setting in which buildings are design and the integrating role of landscape architecture."*

Council stated that the Amendment is consistent with these objectives and strategies because it will minimise the amenity impacts that future development may have on neighbouring properties.

(ii) Local Planning Policy Framework

Council submitted that the Amendment supports the following local planning objectives:

- Clause 21.04-4 (Urban Design) which has the objectives:
 - *to promote a high standard of architectural, landscaping and urban design for built form and public spaces throughout the municipality.*
 - *to control the number of signs and ensure that the appearance, size, illumination or location of signs does not adversely affect the visual amenity of the natural environment or the built form in the municipality.*

The Amendment assists in achieving these objectives by providing design guidance for future development. This will assist in improving the visual amenity of the Shepparton North Commercial Gateway Precinct and the entrance to Kialla Lakes Drive.

(iii) Other planning strategies or policies used in formulating the Amendment

The UDF Addendum states that, in order to achieve desired signage, landscape and built form outcomes for the Kialla Lakes Drive Entrance and the Shepparton North Commercial Gateway Precinct, clear and robust policy framework and guidelines are required.

The Amendment proposes to provide design guidance by amending the existing DDO7 and extending it to the Kialla Lakes Drive Entrance, and by inserting DDO9 into the

Planning Scheme and applying it to the Shepparton North Commercial Gateway Precinct.

2.2 Planning scheme overlays

The purpose of the Design and Development Overlay is *“to identify areas which are affected by specific requirements relating to the design and built form of new development.”* Council submitted that applying DDOs to these areas is the most appropriate planning tool to provide design guidance regarding the built form of new developments.

2.3 Ministerial Directions

Council submitted that the Amendment meets the relevant requirements of:

- Ministerial Direction 11 (Strategic Assessment of Amendments)
- Ministerial Direction on the Form and Content of Planning Schemes under section 7(5) of the Act.

2.4 Discussion

The Amendment introduces built form controls over the northern entrance to Shepparton and extends the area covered by the existing controls to the south. The existing DDO7 and the proposed DDO9 controls will require future development to consider building heights; landscape buffer plantings; front, side and rear setbacks; car parking and signage.

The proposed controls seek, among other things, to ensure that future development minimises large areas of car park paving and provides for large canopy trees and appropriate plant species.

The existing DDO3 and DDO7 provide discretionary guidance over setbacks, height, landscape, signage and design to the southern entrance to the town. Council argued that there is a 'void' in controlling built form, landscaping and signage to the north of Shepparton which is experiencing increased development pressures in addition to significant road infrastructure works. Council submitted that the DDO7 has already been successfully used at the southern entrance to the city.

Council considered that the Amendment completes and entrenches the required built form statutory regime at the northern and southern entrances to the city.

2.5 Conclusion

The Panel considers that the Amendment implements the vision of the UDF Addendum and has support through the State and Local Planning Policy Frameworks, particularly the urban design principles and objectives.

The Panel concludes that the Amendment is supported by, and implements, the relevant sections of the State and Local Planning Policy Framework and is consistent with the relevant Ministerial Directions. The Amendment is well founded and strategically justified, and the Amendment should proceed subject to addressing the more specific issues raised in submissions as discussed in the following chapter.

3 Design and Development Overlay Schedules 7 and 9

3.1 The issue

The issue is whether the UDF Addendum provides the strategic justification for the proposed controls included in the DDO9 and whether changes to the DDO7 are appropriate.

3.2 Submissions

Council informed the Panel that the Amendment affected three precincts:

- The Kialla Park Boulevard Precinct which is centred on the Goulburn Valley Highway south of the city centre
- The Kialla Lakes Drive Entrance abutting the east of the Kialla Park Boulevard Precinct
- The Shepparton North Commercial Gateway Precinct centred on the Goulburn Valley Highway to the north of the city centre.

The Kialla Park Boulevard Precinct (Figure 3) was already covered by the DDO7 and the Amendment proposes to alter the provisions of the Schedule to revise the design and development provisions as recommended by the UDF Addendum. The Amendment proposes to include the Kialla Lakes Drive Entrance in the revised DDO7.

The Shepparton North Commercial Gateway Precinct is proposed to be covered by the new DDO9 as recommended by the UDF Addendum.

Council submitted that the purpose of the Amendment was essentially to improve the entrances to Shepparton and that a clear and robust policy framework was required. Council added:

A UDF prepared by Council in 2006 addressed the southern and eastern entrances to Shepparton but had only limited application along the northern entrance.

The Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017 (the 2017 UDF Addendum) was prepared as something of a 'gap' analysis so as to provide planning scheme guidance for the design and development of land in Kialla and Shepparton North.

Council added that the DDO9 introduces guidance on building design, height and setbacks, fences, parking and access, landscaping and advertising signage.

Council advised that, in response to submissions, it proposed a number of changes to the exhibited DDO7 including:

- identifying properties that may be considered for a lesser setback of not less than 9 metres
- providing that the minimum landscape buffer only applies to commercial properties
- clarifying the wording of the setback requirement

- adding the property at 1 Raftery Road Kialla to the list of properties to which a lesser setback could apply.

The amended DDO7 was submitted to the Panel as Document 5.

Council submitted that the 2006 UDF was prepared in response to development pressure to the south of the town. Council considered the complimentary controls for the northern corridor were overdue.

Council advised the Panel that, following the Directions Hearing on 8 December 2017, it had resolved to remove all the C1Z land from the exhibited DDO9. This change only impacted the land at 177-193 Numurkah Road known as the 'IGA site'. Council submitted that the removal of the C1Z land had addressed the concerns of two submitters with an interest in this land. This view was supported by correspondence between the parties provided to the Panel and the withdrawal of the requests to be heard by both Marl Enterprises Pty Ltd and Shepparton Pty Ltd.

In response to submissions, Council proposed further changes to the controls that impact on land which does not have a frontage to the Goulburn Valley Highway in the DDO9. The amended DDO9 was submitted to the Panel as Document 6 and Council submitted that no further changes were required to the controls.

Council submitted that the controls in the DDO9 were appropriate and robust and had been built on the provisions of existing provisions in the planning scheme, such as Clauses 52.05 – *Advertising Signs* and 52.06 – *Car Parking* to respond to specific issues in Shepparton North.

Alan and Michele Smith advised the Panel that they lived in Reserve Street Kialla and that their concern was with the 20 metre setback in DDO7 and the impact this setback requirement would have on the potential development of their land. Mr and Mrs Smith acknowledged the post-exhibition changes proposed by Council that identified their property and others in Reserve Street where a 9 metre setback may apply. However, they submitted that the word 'may' lacked certainty and should be replaced with 'not greater than'. As part of their submission to the Panel, Mr and Mrs Smith provided a petition signed by a number of the residents of Reserve Street supporting the removal of the 20 metre setback requirement.

Mr Schoonderbeek submitted that he opposed the 20 metre setback in DDO7. He questioned why Reserve Street was included in the Amendment and supported Mr and Mrs Smith's submission on the use of the word 'may'.

Andrew Cleave submitted that his concern was about the setback requirements in the DDO9. He informed the Panel that he was also concerned about the landscaping requirements because the visibility of his landscape supplies business was an important means of advertising. He argued that his land did not have a frontage to the Goulburn Valley Highway and consequently the proposed setback and landscaping controls were inappropriate and unduly onerous.

Wisewould Mahony made a submission on behalf of Marl Enterprises Pty Ltd. Correspondence from Marl Enterprises Pty Ltd acknowledged the post exhibition change proposed by Council to remove the C1Z land from the DDO9. As a consequence, Marl Enterprises advised that it did not wish to pursue its submission.

However, it requested that the Panel take into account its written submissions and the recommendations of the Panel for Amendment C192 and C193.

3.3 Discussion

The UDF Addendum builds on the work implemented in 2006 UDF. Council has had the benefit of over 10 years of applying the document and the associated DDO7. Nevertheless, review of the 2006 UDF is both appropriate and timely. The Panel accepts Council's submission that the UDF addendum was prepared as a gap analysis to identify appropriate planning controls for the design and development of land in Shepparton North and Kialla.

The Panel acknowledges that the UDF Addendum has been subject to public consultation and adopted by Council after the consideration of submissions. The content and recommendations of the UDF Addendum were not contested. The Panel considers it is appropriate as a reference document in the planning scheme and a sound strategic basis for the proposed controls.

The Panel acknowledges that the purpose of the Amendment is to improve the entrances to Shepparton by implementing the recommendations of the UDF Addendum through DDO7 and DDO9 which affect built form and development and not land use.

The Panel notes that the existing DDO7 contains a 20 metre setback requirement with a reduction to 9 metres for a number of properties in Reserve Street. The changes proposed by the Amendment clarify and simplify the explanatory text that details the nature of the exemption but do not alter these existing setback provisions. The controls in the existing and amended DDO7 are discretionary and not mandatory.

In effect, there is no change proposed to the setback provisions affecting Reserve Street. The intent of the controls in DDO7 is to provide guidance and direction which allows for specific development proposals to consider the setback requirements and either comply with them or make the case why a variation is applicable. In this respect, the use of the word 'may' is appropriate and consistent with the approach taken in most development controls.

The Panel accepts post-exhibition proposed additional changes to DDO7. These changes clarify the application of landscaping and parking provisions to non-residential properties and are included in the Panel preferred version of the DDO7 (Appendix D).

At the Hearing, the Panel raised the issue about the last dot point in Clause 5.0 - *Decision Guidelines* in DDO7. This provision states:

Whether the proposal complies with the provisions of DDO2 of this planning scheme to the satisfaction of the Responsible Authority

In the Panel's view, this guideline could have the effect of extending the DDO2 to the area covered by the DDO7. Whether or not a proposal complies with the provisions of the DDO2 is a matter to be considered on land that is covered by that overlay and should not be included as a decision guideline in another overlay. In the Panel's view, the decision guideline is inappropriate and should be removed.

The Panel accepts the proposal by Council to remove the C1Z land from the DDO9 to allow for the development of a Structure Plan or similar for the site. This was a matter considered by the Amendments C192 and C193 Panel which made appropriate recommendations on the matter. The Panel recommended a Structure Plan across the entire area between Ford Road and Hawkins Street, including the IGA site, to guide the broader area into a connected activity centre. A UDF would guide design but not structure of the centre. The Panel notes IGA site forms part of this broader Structure Plan area outlined in the recommendations.

There is no need for the Panel to make any further comment on those recommendations.

In response to submissions, Council made a number of other post-exhibition changes to the exhibited DDO9. These changes clarify the application of landscaping and setback provisions to properties without a frontage to the Goulburn Valley Highway as well as commercial properties with an interface to residential properties. The Panel accepts that these changes are appropriate and address some of the concerns made in submissions. These changes are contained in the Panel preferred version of the DDO9 (Appendix E).

3.4 Conclusions

The Panel concludes:

- The UDF Addendum is an appropriate document for inclusion in Clause 21.09.
- The revised DDO7 (Appendix D) is an appropriate control.
- The revised DDO9 (Appendix E) is an appropriate control.
- The removal of the C1Z land from the DDO9 is appropriate.

3.5 Recommendations

The Panel recommends:

1. **Adopt Amendment C196 to the Greater Shepparton planning Scheme as exhibited subject to the following:**
 - a) **Replace the exhibited maps 13DDO and 14DDO with the revised versions provided in Appendix C.**
 - b) **Replace the exhibited Design and Development Overlay Schedule 7 with the Panel preferred version provided in Appendix D.**
 - c) **Replace the exhibited Design and Development Overlay Schedule 9 with the Panel preferred version provided in Appendix E.**

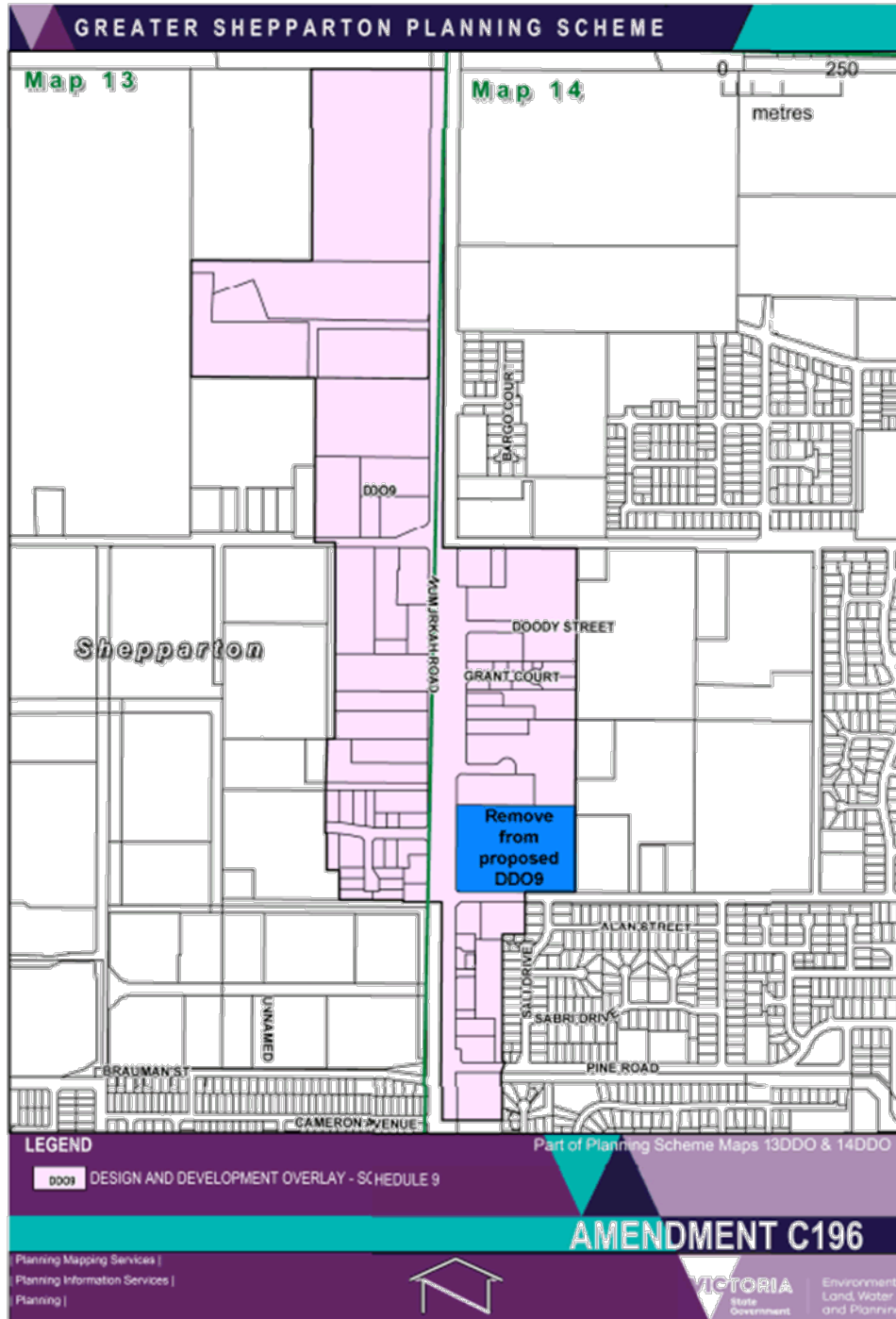
Appendix A Submitters to the Amendment

No.	Submitter
1	Goulburn Murray Water
2	Marl Enterprises Pty Ltd
3	Goulburn Broken Catchment Management Authority
4	Anco Nominees Pty Ltd
5	Gordon and Aileen Youie
6	Department of Environment, Land, Water and Planning
7	Goulburn Valley Water
8	Leo Schoonderbeek
9	Alan and Michele Smith
10	Cleaves Earthmoving and Drainage
11	Country Fire Authority
12	Shepparton Pty Ltd (formerly 18 Pty Ltd)

Appendix B Document list

No.	Date	Description	Tabled by
1	18/01/2018	Submission by Alan and Michele Smith	Ms M Smith
2	18/01/2018	Letters from Greater Shepparton City Council to residents of Reserve Street.	Mr J Keane
3	18/01/2018	Map of the North Commercial Gateway Precinct marking the land Owned by Mr Cleave	Mr A Cleave
4	18/01/2018	Council Part B submission	Mr J Keane
5	18/01/2018	Post-exhibition changes to DDO7	Mr J Keane
6	18/01/2018	Post-exhibition changes to DDO9	Mr J Keane

Appendix C Post Exhibition Mapping Change



Appendix D Panel preferred version of Design and Development Overlay Schedule 7

DD/MM/YYYY
Proposed C196

SCHEDULE 7 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO7**.

KIALLA PARK BOULEVARD PRECINCT

1.0

Design objectives

DD/MM/YYYY
Proposed C196

To create a vibrant and active commercial and business precinct based around large buildings for bulky goods retailing, manufacturing and associated business services within a well-landscaped boulevard setting.

To encourage high quality built form that strengthens and enhances the existing ‘big box’ commercial character of the area, including the provision of front setbacks that can accommodate high quality landscape treatments and the display of goods.

To ensure landscape treatments and signage support and enhance the Goulburn Valley Highway as a key gateway to Shepparton, complement the existing Kialla Lakes Drive and Goulburn Valley Highway landscapes, and reinforce landscaped street edges.

To ensure new development provides clearly defined, legible and attractive gateways to residential areas, particularly at Kialla Lakes Drive, and an appropriate built form and landscape interface between commercial and residential areas.

To implement the design and development guidelines for the Kialla Park Boulevard along the Goulburn Valley Highway and Kialla Lakes Drive in accordance with the *Urban Design Framework – Shepparton North & South Business Areas 2006* and the *Addendum to the Urban Design Framework – Shepparton North & South Business Areas 2017*.

2.0

Buildings and works

DD/MM/YYYY
Proposed C196

Permit requirements

A permit is required to:

- Construct a fence that exceeds 1.2m in height above natural ground level, where the fence is located along a boundary that has a street frontage, and has a transparency ratio of less than 50%, including piers, columns and bases.

Design Requirements

The following buildings and works requirements apply to an application to construct a building or to construct or carry out works, to the satisfaction of the responsible authority:

Building height

- Buildings should be a maximum height above natural ground level as specified in the table below.
- The street wall height of a building facade should not exceed the height above natural ground level specified in the table below.

Precinct (shown on Map 1)	Building height	Street wall height
Goulburn Valley Highway Precinct	14.5m maximum	8m or three storeys

Kialla Lakes Drive Precinct	11m maximum*	6.9m or two storeys*
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Building setbacks

- Building setback should be 20 metres from the front boundary.

~~A lesser~~ The building setback ~~(not less than 9 metres)~~ may be ~~considered by the responsible authority~~ reduced to a minimum of 9 metres from the front boundary for the properties listed below for a ~~single~~ dwelling in a ~~General Residential Zone, Neighbourhood Residential Zone or a Residential Growth Zone.~~

- No. 2 Reserve Street Kialla (Lot No. 1 PS327036F Parish of Kialla)
- No. 4 Reserve Street Kialla (Lot No. 2 PS327036F Parish of Kialla)
- Nos 1/6 and 2/6 Reserve Street Kialla (Lot No. 3 LP63440 Parish of Kialla)
- No. 10 Reserve Street Kialla (Lot Nos. 4 and 5 LP63440 Parish of Kialla)
- No. 12 Reserve Street Kialla (Lot No. 6 LP63440 Parish of Kialla)
- No. 14 Reserve Street Kialla (Lot No. 7 LP63440 Parish of Kialla)
- No. 16 Reserve Street Kialla (Lot No. 8 LP63440 Parish of Kialla)
- No. 1 Raftery Road Kialla (Lot No. 7 LP115007 Parish of Kialla).

A lesser setback (not less than 9 metres) may be considered by the responsible authority for the properties listed below for buildings and works in a Commercial Zone or Industrial Zone.

- Lot 1 and Lot 2 PS433159.
- Lot 1, Lot 2 and Lot 3 PS439192.
- Lot 2 PS426100.
- Lot 1 and Lot 2 LP121036.

Parking and access

- Encourage vehicular access points and driveways to be designed to allow convenient, safe and efficient vehicle movements and connections within the development and to the street network. All vehicles including those delivering to or servicing the site should be able to enter and exit the site in a forward direction.
- A vehicular access point across lot frontage is to be no more than 6.4 metres wide
- Driveways should be designed to minimise any conflict of vehicle movements with pedestrians including consideration of side fences and / or buildings that will impact on sight lines between pedestrians and vehicular traffic.

The following requirements apply to non-residential applications:

- Adequate on site parking in accordance with the Greater Shepparton Planning Scheme should be provided in a form and manner that will not reduce the amenity of the area.
- Encourage developers to consider planting trees throughout car parking areas.

Landscaping

- For interfaces between residential and non-residential properties, A minimum 3 metre landscape buffer must be provided along rear boundaries to ensure suitable interface to residential properties. Where a residential ~~zone~~ property abuts a side boundary of a non-residential property, a minimum 1.5m landscape buffer must be provided.

- **For non-residential applications,** A₁ minimum of 15% of the area to the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree with a mature height of 10 metres (except where the land is also affected by DDO2, when the maximum height should not exceed 7.5 metres, except with a Permit issued pursuant to that Schedule). All plant species must be suitable to this area and to the Council's satisfaction.
- The front building setback should be landscaped to include a variety of shrubs and ground covers. Plant species should be suitable to this area, reflect streetscape planting schemes and be to the satisfaction of the responsible authority.
- Paving materials that provide texture, patterns, subtle colour and permeability to the lot frontage should be used. Large expanses of solid paving such as grey cement or asphalt should be avoided.
- Incorporating small to medium sized trees wherever possible to provide scale, aesthetic relief and shade to front entrances.
- Environmental weeds and invasive tree species should be avoided to the satisfaction of the responsible authority.

Application Requirements

An application for buildings and works must be accompanied by the following information, to the satisfaction of the responsible authority, as relevant:

- Applications within the Sub-Regional Centre (Riverside Plaza) should demonstrate how the proposal will support the site's ongoing function as a key component of the Kialla Lakes Boulevard Precinct.
- A landscaping plan together with proposed irrigation system should be submitted with applications for buildings and works to the satisfaction of the responsible authority.
- The landscape plan and irrigation system should implement water wise, water sensitive urban design and low water use plant materials to the satisfaction of the responsible authority.

Exemption from notice and review

An application for construction of a building or to construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

3.0

Subdivision

DD/MM/YYYY
Proposed C196

None specified.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0

Advertising signs

DD/MM/YYYY
Proposed C196

Advertising sign requirements are at Clause 52.05. All land located within the Kialla Park Boulevard Precinct is in Category 1 – Commercial Areas.

In addition to the relevant requirements at Clause 52.05, an application for advertising signage should meet the following requirements, to the satisfaction of the responsible authority:

- One business identification sign is permitted per development.
- Multiple business occupancies are to share space on the sign.
- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2 metres, and a maximum width of 1.5 metres. This envelope includes the height of any supporting structure.
- Signs attached to a building are encouraged and should be a maximum height of 1.0 metre, and a maximum width of 3.0 metres. Exemptions may be made for signs composed of individual letters that form an integral part of the building façade.
- Panel, Promotion and Major Promotion signs should be avoided.
- Above verandah signs including V-board signs and advertising elements such as banners, flags and inflatable should be avoided.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are electronic, animated, contain moving parts or have flashing elements are not encouraged.
- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.
- Refer to Clause 52.05 of the Shepparton Planning Scheme for general requirements relating to signage.

5.0

Decision guidelines

DD/MM/YYYY
Proposed C196

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the proposal provides for a vibrant and active commercial development that reinforce the character of the area with consistent front setbacks for the display of goods in a well landscaped boulevard setting.
- Whether the height of a proposed building accords with the scale of the local environment and the type of surrounding buildings.
- The effect of the development of proposed buildings on the amenity of abutting buildings.
- Whether the proposed landscape treatment contributes to the character of Kialla Lakes Drive or the Goulburn Valley Highway landscape gateways.
- Whether the design, siting and appearance of buildings improves visual presentation of the frontages at the town entrance.
- The architectural quality and innovative response of the building design.
- Whether building setbacks provided along Kialla Lakes Drive or Goulburn Valley Highway demonstrate appropriate consideration of the streetscape and the residential interface.
- Whether the layout allows for safe access and egress from the site.
- The location of any proposed car parking.
- The inclusion of design elements which protect the amenity of abutting residents.

- Whether the development provides for a robust, suitably scaled landscape treatment that integrates the built form, provides an appropriate scale, reduces its visual impact and provides a suitable interface between the commercial and the residential areas.
- Whether the development provides landscape treatment that visually integrates the commercial areas and contributes to a pleasing streetscape, while also partially screening the buildings when viewed from the road.
- Whether the proposed development contributes to the significance of the Sub-Regional Centre providing specialty services to surrounding residential and business areas.
- Whether the proposed development on land abutting a residential zone complies with the overlooking and overshadowing provisions set out at Clause 54 or Clause 55, as appropriate.
- Whether the design considered energy and resource efficient and sustainable design principles.
- Whether the proposal is in accordance with the design and development guidelines set out in the Urban Design Framework - Shepparton North & South Business Areas, July 2006 and the Addendum to the Urban Design Framework (Planisphere, 2017).
- ~~Whether the proposal complies with the provisions of DDO2 of this planning scheme to the satisfaction of the Responsible Authority.~~

DIAGRAM 1: BUILDING HEIGHT AND SETBACK IN KIALLA LAKES DRIVE PRECINCT

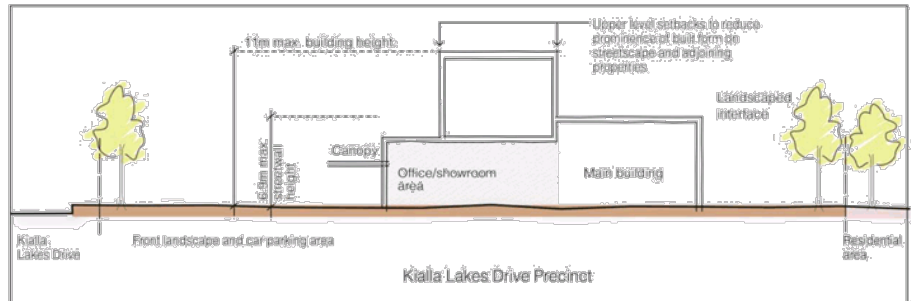


DIAGRAM 2: BUILDING HEIGHT AND SETBACK IN GOULBURN VALLEY HIGHWAY PRECINCT

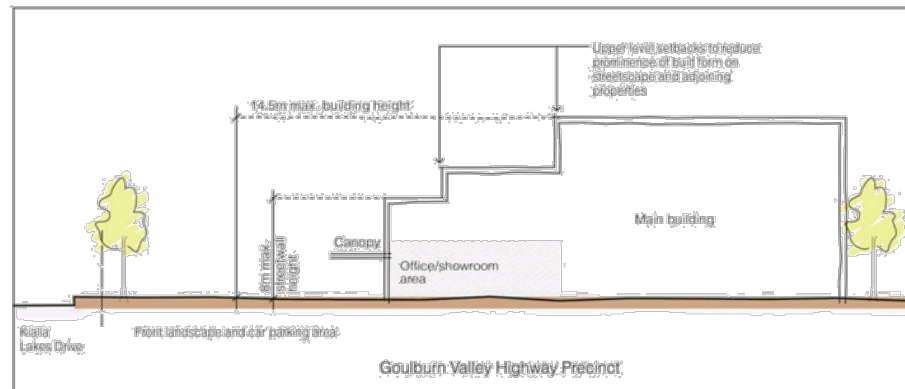


DIAGRAM 3: FREESTANDING BUSINESS IDENTIFICATION SIGN ENVELOPE

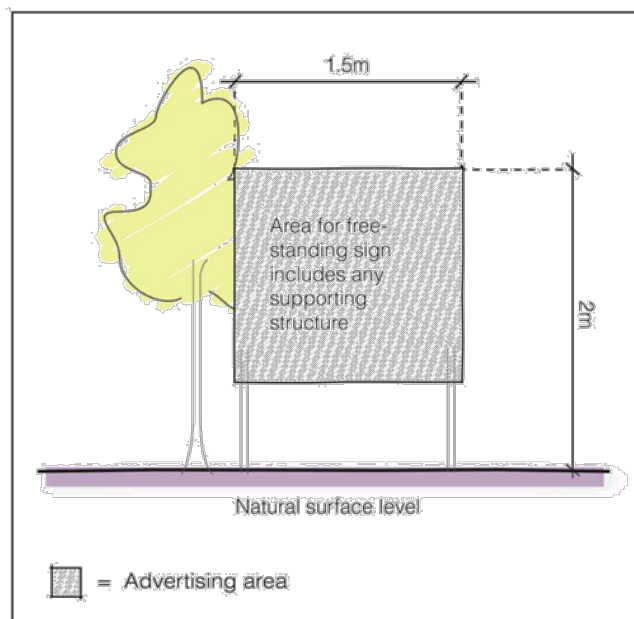
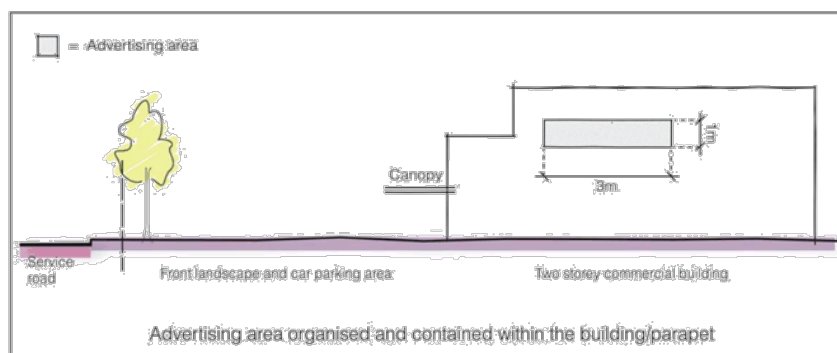
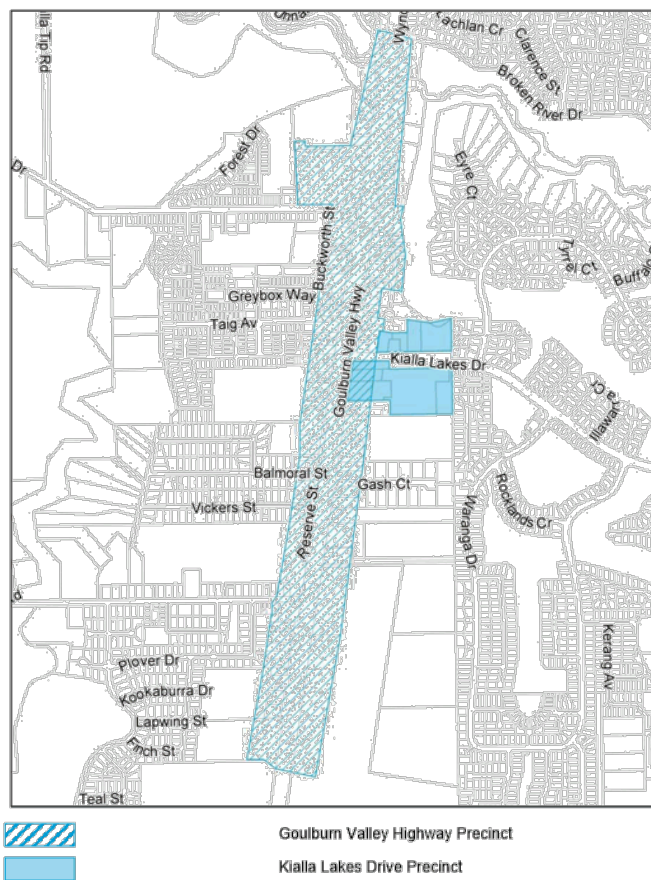


DIAGRAM 4: SIGNS ATTACHED TO BUILDINGS



MAP 1: PRECINCTS



Appendix E Panel preferred version of Design and Development Overlay Schedule 9

SCHEDULE 9 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

DD/MM/YYYY
Proposed C196

Shown on the planning scheme map as **DDO9**.

SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT

1.0 Design objectives

DD/MM/YYYY
Proposed C196

To encourage a vibrant commercial area supporting large scale, regionally significant service and retail activities that reflect the strategic importance of the area as a gateway location in Shepparton's north.

To ensure the design, quality and siting of buildings reflects and enhances the role of the area while making efficient use of the land. This includes at prominent corners and sites at the northern and southern extents of the precinct, where innovative, high quality design is particularly important to strengthen the gateway role of the area.

To ensure signage is appropriately designed, sited and scaled for the gateway corridor context and minimises visual clutter.

To maintain a sense of openness and views to the surrounding landscape corridor from the Goulburn Valley Highway, and integrate public realm planting and a high quality public that appropriately within the surrounding landscape.

To implement the design and development guidelines for the Goulburn Valley Highway in accordance with the *Urban Design Framework – Shepparton North & South Business Areas 2006* and the *Addendum to the Urban Design Framework – Shepparton North & South Business Areas 2017*.

2.0 Buildings and works

DD/MM/YYYY
Proposed C196

A permit is required to:

- Construct a fence that exceeds 1.2m in height above natural ground level, where the fence is located along a boundary that has a street frontage and is:
 - Visually impermeable; or
 - Constructed of unpainted or galvanised steel or uncoated wire.

Design Requirements

The following buildings and works requirements apply to an application to construct a building or to construct or carry out works, to the satisfaction of the responsible authority:

Building design

- The size of new buildings should be in keeping with the large format scale of surrounding development.
- Built form should be articulated and maximise glazing to avoid blank and dominant façades and create an appropriate transition between pedestrian entries, offices and warehouse areas.
- Offices or building areas open to the public should be located at the front of the building massing.
- Upper level building setbacks should be used to reduce building bulk and add visual interest.
- Street setbacks must incorporate significant landscaping and sealed car parking, and be utilised to provide clear sightlines and legibility to the street.
- Building roof form should be designed to complement the prevailing character.

- Plant equipment, vents and any other mechanical equipment must be carefully designed and sited or incorporated into the roof design, to ensure it is screened or concealed from the street, surrounding public spaces and buildings.
- Where possible, ensure verandahs and canopies are incorporated into single storey built form components to enhance site amenity and provide shade for employees and visitors to the site.
- Sustainable water use in buildings should be provided by implementing measures to collect rain water runoff from roof areas, and ensure water storage/tanks are located away from public view, and do not impact on neighbours' visual amenity.
- Materials for external surfaces of new developments should consider and complement those used in existing buildings in the area, such as brick and non-reflective, high quality cladding.
- The use of concrete wall construction (e.g. tilt panels) should be complemented by the use of contrasting claddings such as timber and metals that provide facade definition.
- Reflective glass, PVC, unrelieved painted render, highly reflective materials such as zincalume, unarticulated concrete surfaces and unarticulated cladding systems should be avoided. Bright, extravagant colours schemes should also to be avoided.
- Colour schemes of all external surfaces of new developments must complement those found in the surrounding area and should consider neutral colours.

Building height

- Buildings should be a maximum height of 11m above natural ground level, plus any applicable flood level determined by a relevant referral authority pursuant to this scheme. A building may exceed the maximum height by up to 1 metre if the slope of the natural ground level, measured at any cross section of the site of the building wider than 8 metres, is greater than 2.5 degrees. In that case, the building height should be 10 metres plus any applicable flood level determined by a relevant referral authority pursuant to this scheme. See Diagram 3.
- The street wall height of a building frontage should not exceed two storeys (or 6.9m) above natural ground level. See Diagram 3.

Building setbacks

- Building setback should be a minimum of 9m and maximum of 20m from the front lot boundary, to the satisfaction of the responsible authority. [A lesser setback may be considered by the responsible authority for lots that do not abut a Road Zone Category 1.](#) See Diagram 4.
- ~~For interfaces between residential and non-residential properties~~ ~~Where a zero (0m) setback is sought on land adjacent to the GRZ1, the overshadowing and overlooking provisions of Clause 55 the Greater Shepparton Planning Scheme must be considered to limit impacts of commercial development on residential amenity with regard to overshadowing and overlooking.~~
- Where possible, development should be set back from at least one side boundary by a minimum of 1m to enable views from the Goulburn Valley Highway between buildings to the surrounding area.
- ~~For lots that abut a Road Zone Category 1.~~ Loading and unloading areas should be set back a minimum distance of 20m from the front lot boundary.

Fences

- Fencing should be constructed from materials that complement the building and surrounding area and should be painted in muted colours/tonings, avoiding galvanised steel mesh and 'Colorbond' style fencing.
- Front fences should have a minimum 50% visual permeability.
- Where possible, fencing should be softened and screened by vegetation planting.

Parking and access

- Sufficient car parking spaces should be provided on site for occupants and visitors in accordance with Clause 52.06 of the Greater Shepparton Planning Scheme.

- For larger sites, consideration should be given to short-term parking areas located in the front setback, with longer-term parking (i.e. staff parking) located to the rear.
- Avoid large paved car parking areas that are not broken up by landscaping.
- Ensure a landscape buffer is provided between car parking areas and the front property boundary.
- Where practicable, use landscape design to provide significant canopy trees that can assist in shading car parking areas to mitigate heat impacts.
- Where practicable, limit vehicle entry points to one crossover and avoid large areas of circular driveway aprons and impermeable surfaces.
- Group access points and limit double crossover widths to large sites to minimise opportunities for vehicle conflict between ~~the service roads~~ and [a Road Zone Category 1GVH](#).
- For corner sites, encourage vehicle access to a side road rather than the [Goulburn Valley Highway](#) service roads.
- For the purpose of providing an active street frontage, not more than 7.5m on any site frontage should be utilised for vehicular access purposes. See Diagram 6.

Landscaping

- ~~A minimum 1.5m landscape buffer must be established along rear boundaries to ensure suitable interface with adjoining residential lots in the GRZ1. See Diagram 5.~~
- ~~For interfaces between residential and non-residential properties, a~~ minimum 1.5m landscape buffer should be established to ~~ensure suitable interface with all other adjoining zones~~ ~~limit impacts of commercial development on residential amenity.~~ See Diagram 5
- A minimum of 15% of the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree (mature height 10m). Plant species must be appropriate and suitable to this area, and to Council's satisfaction.
- Provide large canopy trees and understorey planting to minimise loss of views from the [Goulburn Valley Highway](#) corridor to the surrounding landscape.
- Minimise areas of paving, particularly in the front setback.

Advertising signage

- Panel, Promotion and Major promotion signs will only be considered acceptable on a temporary basis (e.g. a maximum of 15 years from the date of permit issue).
- Free-standing signage (e.g. pole signs) must be set back a minimum distance of 1m from the front property boundary.
- Signage, including painted signs, located on a side wall should not occupy more than 50% of the area of the wall.
- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2m and maximum width of 1.5m. This envelope includes the height of any supporting structure. See Diagram 1.
- Signs attached to a building are encouraged and should be a maximum height of 1m and a maximum width of 3m. Exemptions may be made for signs composed of individual letters that form and integral part of the building facade. See Diagram 2.

Application Requirements

An application for buildings and works must be accompanied by the following information, to the satisfaction of the responsible authority, as relevant:

- A written response detailing how the proposal appropriately responds to the objectives and provisions of the Schedule.

- A detailed landscape plan and irrigation system demonstrating how the development will implement water wise, water sensitive urban design and low water use plan materials, to the satisfaction of the responsible authority.

An application to vary the requirements set out in a Sub-clause to this schedule must provide a written submission demonstrating how the proposal responds to the strategic **importance and gateway-significance** of the **area as a gateway corridor**, while providing:

- Innovative, high quality design;
- Efficient use of the land; and
- Enhancement of the appearance and strengthening the identity of the commercial corridor.

This requirement applies to proposals on prominent sites such as corners and sites at the northern and southern extents of the precinct.

Exemption from notice and review

An application for construction of a building or to construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

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Subdivision

None specified.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

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Advertising signs

Advertising sign requirements are at Clause 52.05. All land located within the Shepparton North Gateway Commercial Area Precinct is in Category 1 – Commercial Areas.

In addition to the relevant requirements at Clause 52.05, an application for advertising signage should meet the following requirements, to the satisfaction of the responsible authority:

- Business identification signage should form an integral part of the front building façade, be appropriately proportioned to sit within the parapet or gable end, and not protrude above or beyond the building façade.
- Business identification information can be incorporated into one way-finding oriented sign at the entrance to each section of service road to reduce visual clutter.
- Major promotion signs should be attached to and integrated into the front building facade.
- High wall and sky signs are discouraged.
- Panel and Promotion signs that project outside the perimeter of the building, or are free-standing, are discouraged.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are electronic, animated, contain moving parts or have flashing elements are not encouraged.
- Bunting, animated and reflective signs, including sandwich boards and inflatable and temporary signs, are discouraged.

- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.

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Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The design and development guidelines set out in the *Addendum to the Urban Design Framework - Shepparton North & South Business Areas* (Planisphere, 2017).

Building design and siting

- Whether the height of a proposed building accords with the scale of the local environment and type of surrounding buildings.
- The design and siting of existing development in the area.
- The interface with development on adjoining zones outside the Commercial 1 Zone ~~(C1Z)~~ or Commercial 2 Zone ~~(C2Z)~~.
- Whether the proposed development appropriately responds to and enhances the character of the *Shepparton North Gateway Commercial Precinct* and its gateways.
- Whether the proposed front setbacks enable efficient use of the land.
- Whether opportunities for views between buildings to the surrounding area are available.
- Whether there is any potential for land use conflicts with adjoining zones as a result of the proposed building siting.
- Whether the development has been designed in a manner that is climatically appropriate for the local context.
- The potential for the proposed building to impact visual amenity of surrounding lots.
- Whether the proposed building incorporates active and attractive elements that interact with the streetscape.
- Whether, in seeking to vary any requirements in this Schedule, the proposed development:
- Achieves innovative, high quality design and efficient use of the land; and
- Enhances the appearance and strengthens the identity of the commercial corridor.

Landscaping

- Whether adequate space has been provided for landscaping within the front setback.
- Whether plant choice is sustainable and suitable within the local environment.
- Where front fencing is proposed, whether the proposed fence enhances the street appearance and maintains view to, between, and around sites.
- Whether the proposed landscape concept plan responds to Council's Landscape Plan Guide (Spiire, 2017) and any relevant planting schedule.

Advertising signage

- Whether the proposal is appropriately designed, sited and scaled for the Goulburn Valley Highway corridor without detracting from the public realm or competing with way-finding and road signage.
- Whether colours and materials proposed might interfere with the safety or efficiency of the Goulburn Valley Highway as a Road Zone, Category 1 (RDZ1).

Diagram 1: Freestanding business identification sign envelope

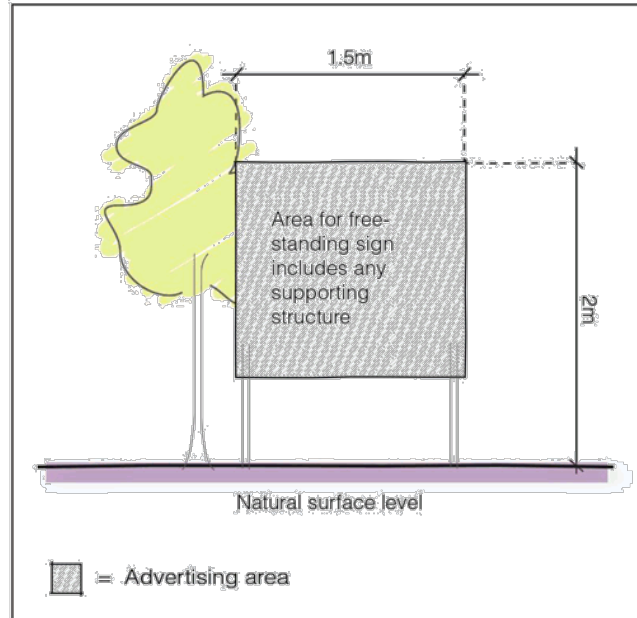


Diagram 2: Signs attached to buildings

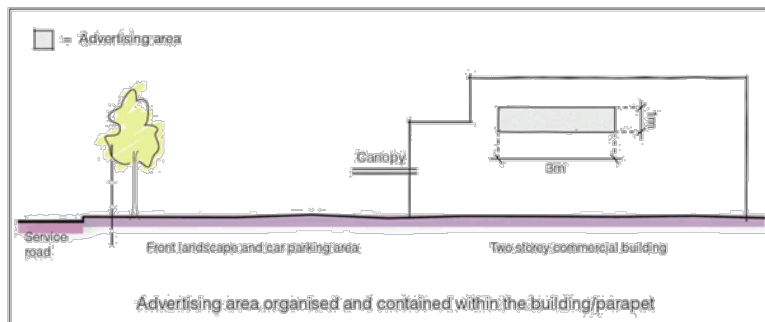


Diagram 3: Building heights and setbacks

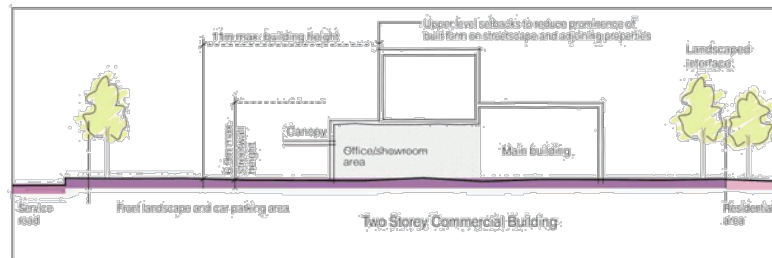


Diagram 4: Site layout and design

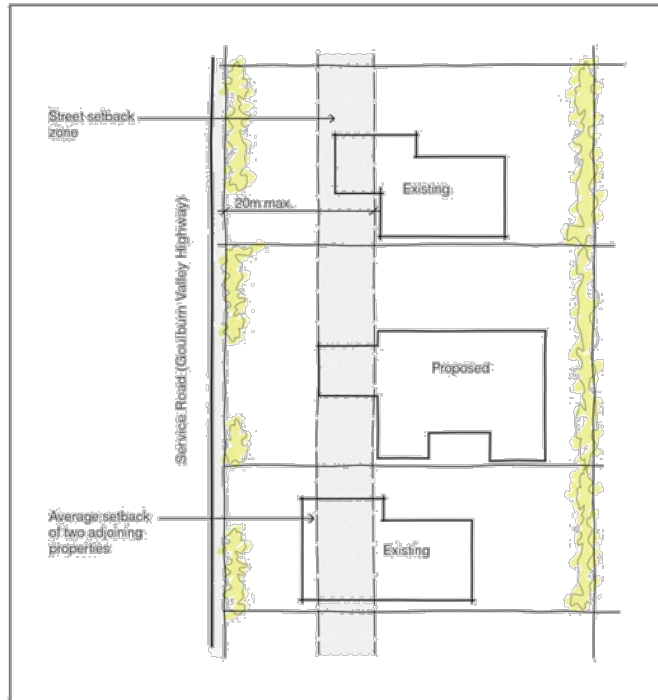
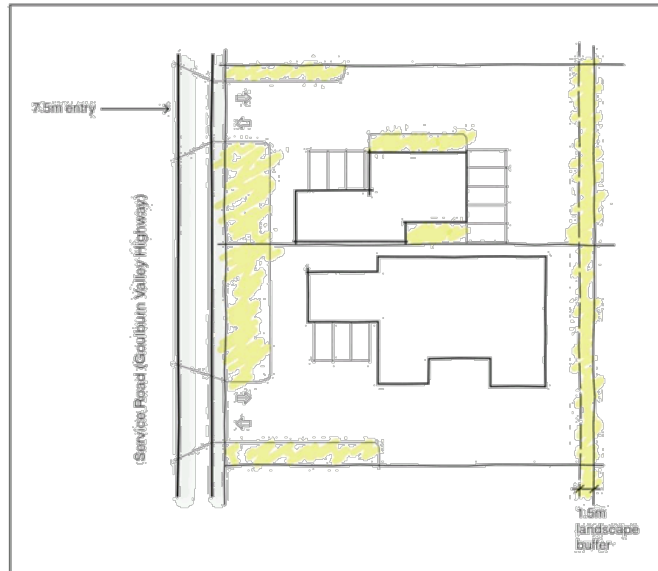
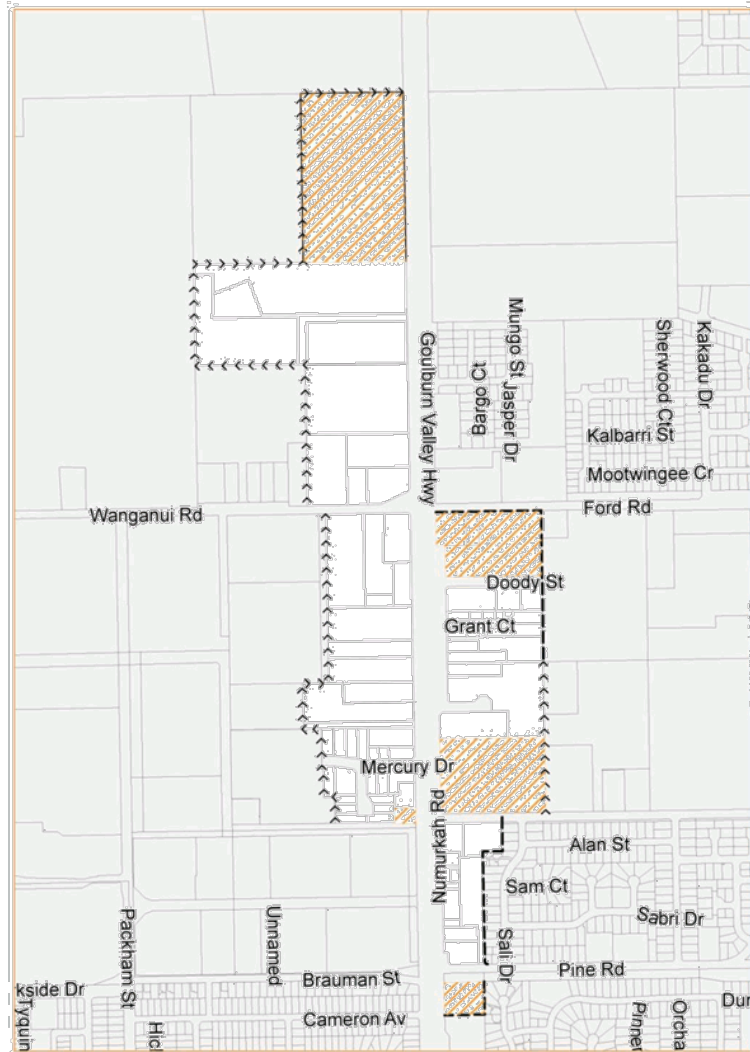







Diagram 5: Landscape buffer



Map 1: Framework Plan



Legend

-  Land outside subject area
-  Prominent sites (including gateways)
-  Property boundaries
-  Interface with adjoining residential-zoned land
-  Interface with other non-commercially-zoned land



Planning and Environment Act 1987

GREATER SHEPPARTON PLANNING SCHEME

AMENDMENT C196

EXPLANATORY REPORT

Who is the planning authority?

This amendment has been prepared by the Greater Shepparton City Council, which is the planning authority for this amendment.

The Amendment has been made at the request of the Greater Shepparton City Council.

Land affected by the Amendment

The Amendment applies to three main areas of land, being:

1. Kialla Park Boulevard Precinct is highlighted in purple on *Figure 1 – Kialla Park Boulevard Precinct*. This is land within Schedule 7 to Clause 43.02 – Design and Development Overlay (DDO7) and generally includes land on both sides of the Goulburn Valley Highway between Broken River Drive at the northern extent and the Shepparton Aerodrome at the southern extent. The Amendment makes changes to DDO7, which applies to this land.
2. Kialla Lakes Drive Entrance is highlighted in blue in *Figure 2 – Kialla Lakes Drive Entrance*. This generally includes land at both sides of the entrance to Kialla Lakes Drive from the Goulburn Valley Highway. The Amendment extends the DDO7 (shown in *Figure 2* hatched in blue) onto this land.
3. Shepparton North Commercial Gateway Precinct is highlighted in pink in *Figure 3 – Shepparton North Commercial Gateway Precinct*. This generally includes land on both sides of the Goulburn Valley Highway (Numurkah Road) within the Commercial 2 Zone from immediately south of the Pine Road intersection at the southern extent to the commercial land north of the Wanganui Road intersection at the northern extent. The Amendment applies a new Schedule 9 to the Design and Development Overlay (DDO9) to this land.

Specifically, the Amendment applies to all land parcels described in Attachment 1 to this explanatory report.

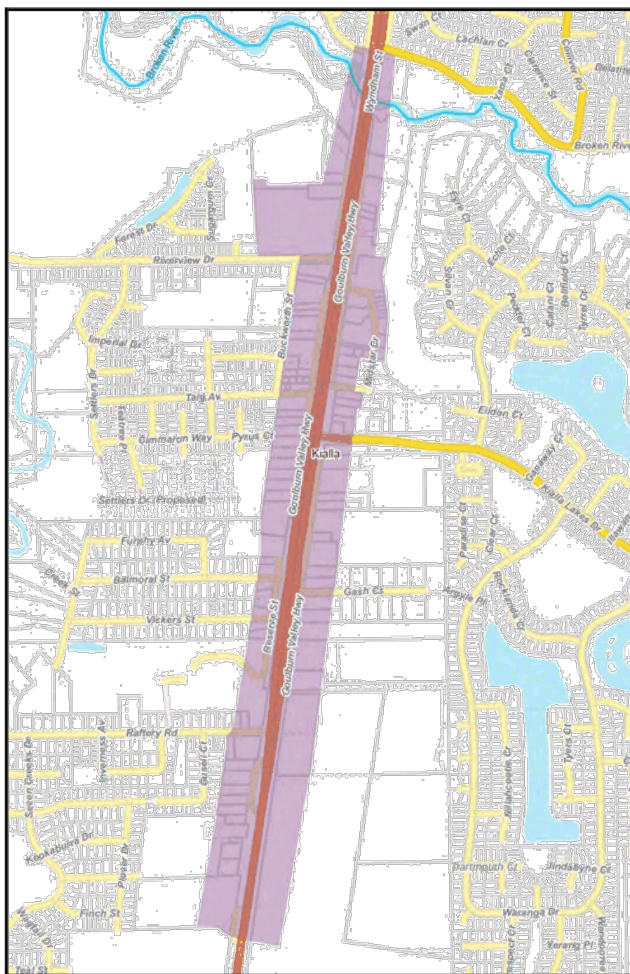


Figure 1 – Kialla Park Boulevard Precinct

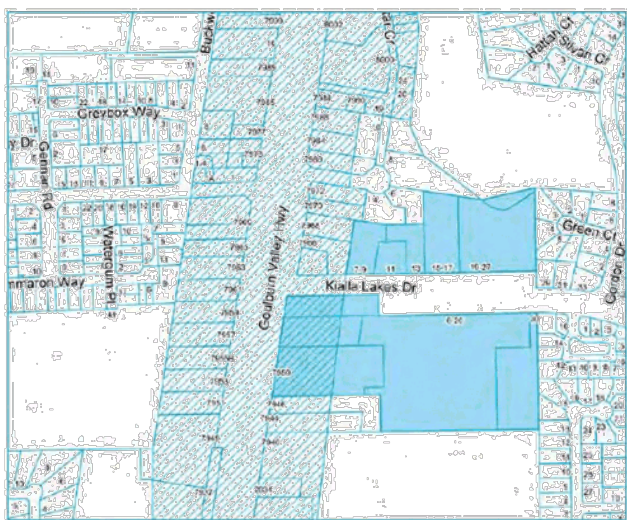


Figure 2 – Kialla Lakes Drive Entrance (properties to be added to existing DDOT in blue)



Figure 3 – Shepparton North Commercial Gateway Precinct

What the amendment does

The Amendment introduces the *Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017* (UDF Addendum) to the Planning Scheme as a reference document and implements its findings to provide guidance for the design and development of land in Kialla and Shepparton North. Additionally, the Amendment ensures that the DDO7 content aligns with the new *Ministerial Direction on the Form and Content of Planning Schemes*.

Specifically, the Amendment:

- Amends Clause 21.09 (Reference Documents) to include the *Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017* as a reference document;
- Applies Schedule 7 to Clause 43.02 (Design and Development Overlay) to land at the Kialla Lakes Drive Entrance;
- Amends Schedule 7 to Clause 43.02 (Design and Development Overlay) to revise the design and development guidance in this Schedule;
- Applies Schedule 9 to Clause 43.02 (Design and Development Overlay) to land in Shepparton North;
- Inserts a new Schedule 9 to Clause 43.02 (Design and Development Overlay) to implement the recommendations of the *Addendum to the Urban Design Framework: Shepparton North and South Business Areas June 2017* for the Shepparton North Gateway;
- Inserts new Planning Scheme Map Nos. 13DDO and 14DDO;
- Amends Planning Scheme Map No. 26DDO; and
- Amends the Schedule to Clause 61.03.

Strategic assessment of the Amendment

Why is the Amendment required?

The Amendment is required to introduce the *Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017* (the UDF Addendum) to the Planning Scheme as a reference document and implement its recommendations to provide guidance for the design and development of land in Kialla and Shepparton North.

The UDF Addendum states that, in order to achieve desired signage, landscape and built form outcomes for the Kialla Lakes Drive Entrance and the Shepparton North Commercial Gateway Precinct, clear and robust policy framework and guidelines are required.

The Amendment provides this design guidance by amending the existing DDO7 and extending it to the Kialla Lakes Drive Entrance, and by inserting DDO9 into the Planning Scheme and applying it to the Shepparton North Commercial Gateway Precinct.

The Amendment also ensures that the DDO7 content is consistent with the Ministerial Direction on the Form and Content of Planning Schemes.

How does the Amendment implement the objectives of planning in Victoria?

The Amendment provides design guidance for any future development of two key areas in Shepparton, being the Shepparton North Commercial Gateway Precinct, and the entrance to Kialla Lakes Drive. This assists in providing for the fair and orderly use and development of land, which implements objectives (a), (c), (f) and (g) of Section 4(1) of the *Planning and Environment Act 1987* (the Act).

How does the Amendment address any environmental, social and economic effects?

Environmental effects

The design and development controls that the Amendment applies require that future development considers landscape buffer plantings around front, side and rear setbacks. These controls have positive environmental impacts as they ensure that future development minimises large areas of paving, and provides large canopy trees and appropriate plant species. The UDF Addendum includes references to Council's *Landscape Plan Guide, 2017* and requires consideration of this document as a decision guideline within the new Schedule 9 to the Design and Development Overlay.

There are no adverse environmental impacts.

Social effects

The Amendment provides design guidance for built form in the Shepparton North Commercial Gateway Precinct and the entrance to Kialla Lakes Drive. This includes guidance for building design, height and setbacks, fences, parking and access, landscaping and advertising signage. This improves visual amenity in these areas and achieves positive social outcomes.

There are no adverse social impacts.

Economic effects

The Amendment has positive economic impacts as it provides clear guidance on design expectations for built form in the Shepparton North Commercial Gateway Precinct and the entrance to Kialla Lakes Drive. This helps to avoid uncertainty and inconsistency for developers within these areas.

There are no adverse economic impacts.

Does the Amendment address relevant bushfire risk?

The Amendment does not increase the risk of life, property, community infrastructure and the natural environment from bushfire.

Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?

The amendment complies with the Ministerial Direction on the Form and Content of Planning Schemes issued under section 7(5) of the Act.

How does the Amendment support or implement the State Planning Policy Framework and any adopted State policy?

The State Planning Policy Framework objective of Clause 15.01-2 (Urban design principles) is *“to achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm while minimising detrimental impact on neighbouring properties”*.

A strategy of Clause 15.01-2 (Landscape architecture) is that *“recognition should be given to the setting in which buildings are design and the integrating role of landscape architecture”*.

The Amendment is consistent with these objectives and strategies as it minimises the amenity impacts that future development may have on neighbouring properties.

How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?

The Amendment is consistent with the Local Planning Policy Framework. In particular, it is supportive of and assists in the implementation of elements of the Municipal Strategic Statement as follows:

Two objectives of 21.04-4 (Urban Design) are *“to promote a high standard of architectural, landscaping and urban design for built form and public spaces throughout the municipality”* and *“to control the number of signs and ensure that the appearance, size, illumination or location of signs does not adversely affect the visual amenity of the natural environment or the built form in the municipality”*.

The Amendment assists in achieving these objectives by providing design guidance for future development. This ensures that future development improves the visual amenity of the Shepparton North Commercial Gateway Precinct and the entrance to Kialla Lakes Drive.

Does the Amendment make proper use of the Victoria Planning Provisions?

The purpose of the Design and Development Overlay is *“to identify areas which are affected by specific requirements relating to the design and built form of new development”*.

Applying DDOs to these areas is the most appropriate planning tool to provide design guidance regarding the built form of new developments.

The Amendment makes proper use of the Victorian Planning Provisions.

How does the Amendment address the views of any relevant agency?

During exhibition, notice was issued to all relevant referral authorities in accordance with the Act. No agencies raised any objections to the amendment.

Does the Amendment address relevant requirements of the Transport Integration Act 2010?

The purpose of the *Transport Integration Act 2010* is to create a new framework for the provision of an integrated and sustainable transport system in Victoria. The vision statement recognises the aspirations of Victorians for an integrated and sustainable transport system that contributes to an inclusive, prosperous and environmentally responsible State.

The objectives of the *Transport Integration Act 2010* relate to social and economic inclusion, economic prosperity, environmental sustainability, integration of transport and land use, efficiency, coordination and reliability, safety, and health and wellbeing.

Given that the Amendment provides design guidance, the impact on the transport system will be negligible, as defined by Section 3 of the *Transport Integration Act 2010*.

The Minister has not prepared any statements of policy principles under Section 22 of the *Transport Integration Act 2010*; therefore, no such statements are applicable to this proposed Amendment.

Resource and administrative costs

- **What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

The Amendment may result in a small increase in the number of permit applications to be assessed by Council. This will not cause a significant or unreasonable burden on the resource and administrative costs of the responsible authority.

Where you may inspect this Amendment

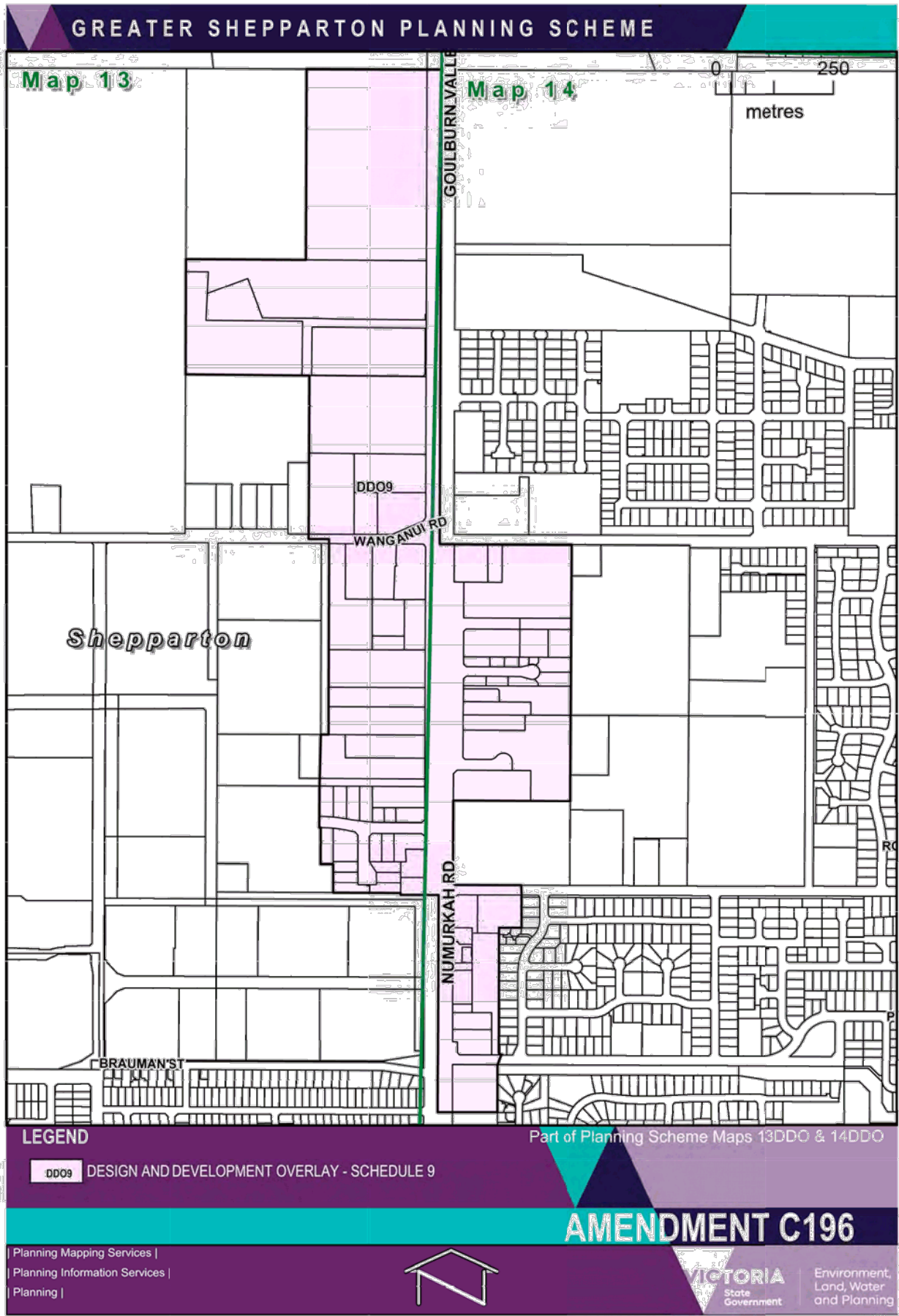
The Amendment is available for public inspection, free of charge, during office hours at Greater Shepparton City Council, 90 Welsford Street, Shepparton.

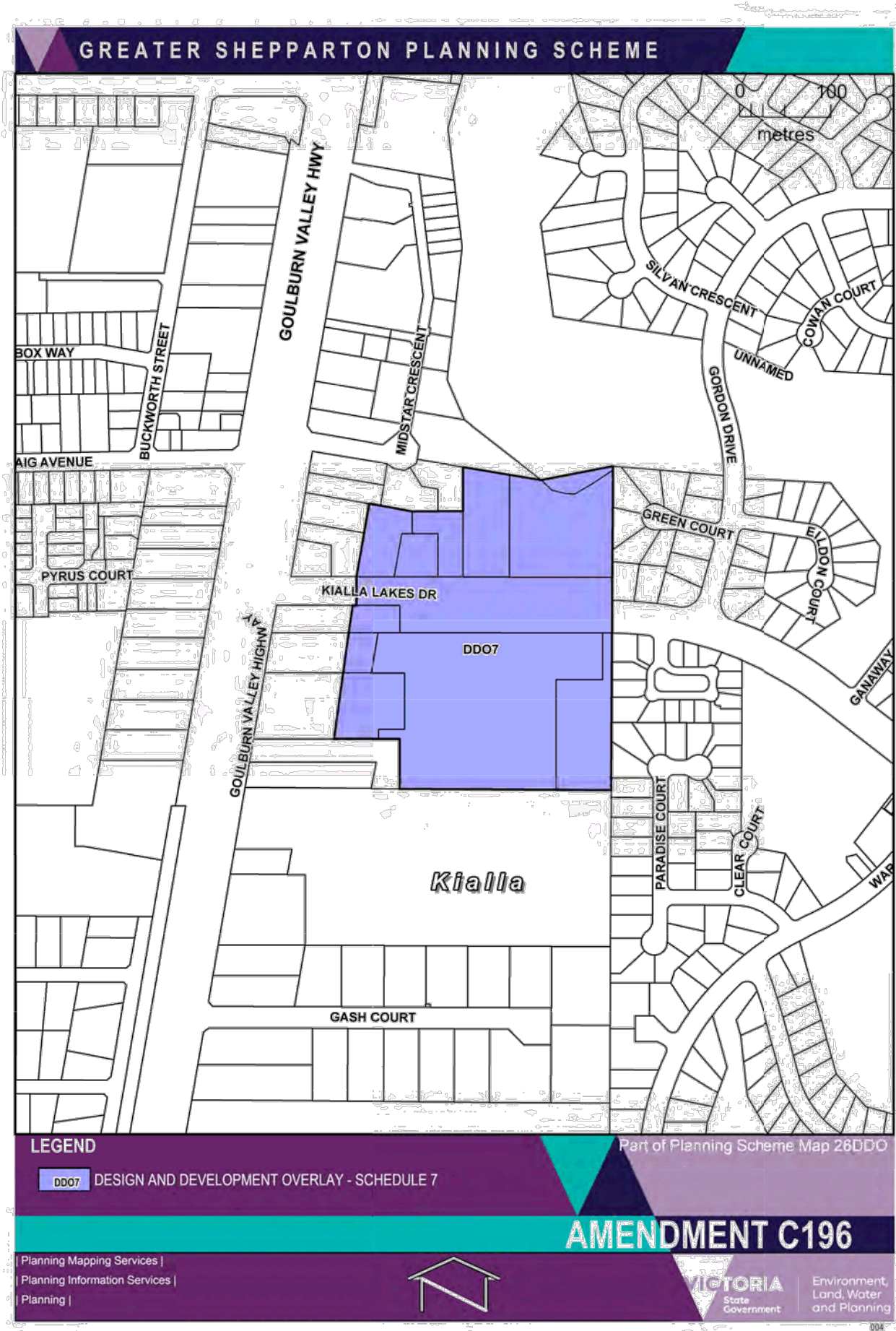
The Amendment can also be inspected free of charge at the:

- Department of Environment, Land, Water and Planning website at www.delwp.vic.gov.au/public-inspection; and
- Greater Shepparton City Council website at www.greatershepparton.com.au.

ATTACHMENT 1 – LAND AFFECTED BY THE AMENDMENT

Land within the existing Design and Development Overlay 7 (DDO7) – Kialla Park Boulevard Precinct that is proposed to be revised:
2 Broken River Drive, Shepparton
2-100 Lincoln Drive, Shepparton
2, 4, 6, 10, 12, 14, 16, 18 and 20 Reserve Street, Kialla
1, 1A, 2 and 4 Raftery Road, Kialla
26 and 26A Riverview Drive, Kialla
1 and 6-20 Kialla Lakes Drive, Kialla
1 and 1A Taig Avenue, Kialla
1/2, 1/4, 2/4, 6, 8 and 16 Buckworth Street, Kialla
19, 20, 24, 30-32, 34-36, 38, 40, 42, 44, 46 and 48 Midstar Crescent, Kialla
7805, 7810, 7815, 7821, 7825, 7831, 7833, 7835, 7845, 7847, 7851, 7853, 7855, 7879A, 7880, 7880A, 7882, 7896, 7900, 7910, 7920, 7922, 7924, 7926, 7930, 7934, 7937, 7940, 7944, 7946, 7949, 7951, 7953A, 7953B, 7955A, 7955B, 7957, 7957B, 7959, 7950, 7954, 7958-7960, 7961A, 7961B, 7963, 7965, 7966, 7967, 7968, 7969, 7970, 7971, 7972, 7973, 7975, 7980, 7982, 7984, 7986, 7988, 7989, 7990, 7993, 7993A, 7995, 7999, 8000, 8001, 8002, 1/8005, 2A/8005, 2B/8005, 8006, 1/8009, 2/8009, 8010, 8012, 8018, 8020, 8022, 8024, 8025, 8026, 8028, 8032, 8038, 8040, 8041, 8042, 8043, 1/8045, 2/8045, 3/8045, 8048, 8049, 8050, 8050A, 8053 and 8060 Goulburn Valley Highway, Kialla
Land being included within the existing Design and Development Overlay 7 (DDO7) – Kialla Park Boulevard Precinct:
6-20, 7-9, 11, 13, 19-27 and 30 Kialla Lakes Drive, Kialla
7954 and 7958-7960 Goulburn Valley Highway, Kialla
Land being included within Design and Development Overlay 9 (DDO9) – Shepparton North Commercial Gateway Precinct:
10 Ford Road, Shepparton
1-17 and 8-12 Hawkins Street, Shepparton
1, 2, 3, 4, 5, 6, 7 and 8 Grant Court, Shepparton
295, 295A, 295B, 375, 405, 433, 435, 437, 441, 443 and 445-447 Goulburn Valley Highway, Shepparton North
Shelby Court, Shepparton
1, 2, 3, 4, 5 and 6 Neptune Court, Shepparton
111-125, 127-137, 139, 141, 143, 145-155, 159, 161, 169-175, 176, 177-193, 178-180, 182-184, 186-188, 192, 192A, 194-196, 195, 198-200, 202, 205, 208-210, 212-216, 207, 209, 211, 213, 215, 219, 221-229, 228-238 and 231-237 Numurkah Road, Shepparton
3, 3A, 4, 5, 6, 7, 8-10, 9, 11 and 12 Mercury Drive, Shepparton
1-7 and 9 Pine Road, Shepparton
15, 17 and 25 Wanganui Road, Shepparton
2, 16 and 20 Wanganui Road, Shepparton North





GREATER SHEPPARTON PLANNING SCHEME

21.09

06/04/2016

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REFERENCE DOCUMENTS

Settlement

[Addendum to the Urban Design Framework: Shepparton North and South Business Areas July 2017](#)

Encouraging Arts in the Community, City of Greater Shepparton

Greater Shepparton 2030 Strategic Report Strategy Plan 2005

Greater Shepparton Housing Strategy, David Lock Associates, 2011

Mooroopna West Growth Corridor Structure Plan, Maunsell Australia, January 2013

Recreation and Open Space Strategy, City of Greater Shepparton, 1998

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South Shepparton Community Infrastructure Needs Assessment 2011

Technical Notes – Urban Design Specifications, City of Greater Shepparton

Urban Design Framework, City of Greater Shepparton, March 1999

Urban Design Framework – Shepparton North and South Business Areas

Environment

Best Policy and Practice Guidelines for Dryland Irrigation in Dryland Catchments, Goulburn Broken Catchment Management Authority, 2001

Biodiversity Map, Department of Natural Resources and Environment

Catchment and Land Protection Act, 1994

'City of Greater Shepparton Heritage Study Stage Two', Allom Lovell and Associates, 2003

Crown Land Standard Planning Permit Conditions, DSE 2003

Draft Goulburn Broken Catchment Water Quality Strategy, Goulburn Broken Catchment Management Authority, 2003

Floodplain Management Guidelines for Whole Farm Plans, Goulburn Broken CMA

Goulburn Broken Catchment Management Authority By Law 1 Waterways Protection

Goulburn Broken Catchment Strategy, Goulburn Broken CMA, 2003

Goulburn Broken Catchment Vegetation Management Strategy, Goulburn Broken CMA

Goulburn Broken Nutrient Management Strategy, Goulburn Broken CMA

Goulburn Broken Regional Floodplain Management Strategy, Goulburn Broken CMA 2002

Heritage Rivers Act 1992

Greater Shepparton Heritage Study Stage IIB, Heritage Concepts, May 2013

Land Capability Assessment for Onsite Domestic Wastewater Management, EPA Publication 746.1, 2003

Protection of Water Quality Guidelines, North East Planning Referral Group, 2001

GREATER SHEPPARTON PLANNING SCHEME

Review of Buffer Distances Surrounding Wastewater Management Facilities, 2002, undertaken by Urban and Regional Planning for Goulburn Valley Water

Septic Tanks Code of Practice, EPA publication 891, 2003

Shepparton Floodplain Management Plan, Greater Shepparton City Council, 2002

Shepparton Irrigation Region Land and Water Salinity Management Plan, Department of Natural Resources and Environment, 1989 and 1995 review

Shepparton Irrigation Region Surface Drainage Strategy, Goulburn Murray Water, June 1995

Victoria's Biodiversity – Directions in Management, DNRE 1997

Victoria River Health Strategy, DNRE, 2002

Water (Irrigation Farm Dams) Act, 2002

Economic Development

Shepparton CBD Strategy, October 2008

Campaspe, Greater Shepparton and Moira Regional Rural Land Use Strategy, 2008

Earthworks Controls in the Shepparton Irrigation Region – Discussion and Options Paper, August 2010

Goulburn Murray Waters Regional Tourism Plan, Tourism Victoria, 1997

Goulburn Valley Freight Logistics Centre Study, Freight Logistics Bureau

Greater Shepparton - Australia's Taste Sensation, Shepparton Tourism Plan, City of Greater Shepparton, July 1997

Greater Shepparton Planning Scheme Strategic Review of Tatura Industrial Land, Keaney Planning, June 2011

Greater Shepparton Regional Rural Land Use Strategy Issues Paper, 2009

Industrial Development Guidelines, City of Greater Shepparton, December 1998

Industrial Land Review, City of Greater Shepparton, 2011

Interim Guidelines for Irrigation Development in the Goulburn Murray Region (Version 4), Goulburn Murray Water, 17 November 1998

Shepparton Landscape and Urban Design Framework, City of Greater Shepparton, March 1997

Shepparton North and South Growth Corridors, Outline Development Plan, Spiire Pty Ltd, April 2014

Shepparton Region Industrial Development Strategy, Shepparton-Kyabram-Rodney Development Corporation, December 1994

Infrastructure

Community Surface Drainage Schemes – Guidelines for Design, Community Surface Drainage Co-ordinating Committee, March 1997

Greater Shepparton Bicycle Strategy Review, PBAI Australia, 2006

Infrastructure Design Manual (as revised), Local Government Infrastructure Design Association

Municipal Transport Plan, City of Greater Shepparton, December 1998

GREATER SHEPPARTON PLANNING SCHEME

Regional Waste Management Plan, Goulburn Valley Regional Waste Management Group, July 1998

Roadside Management Plan, City of Greater Shepparton, 1999

Shepparton Bypass Planning Study Report, Ove Arup & Partners, 1998

Significant Drainage Lines Map series, Goulburn Murray Water and Goulburn Broken Catchment Management Authority, July 1998

Surface Drainage Feasibility Study, Guilfus Congupna Community Drainage Group, December 1992

GREATER SHEPPARTON PLANNING SCHEME

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SCHEDULE 7 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO7**.

KIALLA PARK BOULEVARD PRECINCT

1.0
DD/MM/YYYY
Proposed C196

Design objectives

To create a vibrant and active commercial and business precinct based around large buildings for bulky goods retailing, manufacturing and associated business services within a well-landscaped boulevard setting.

To encourage high quality built form that strengthens and enhances the existing 'big box' commercial character of the area, including the provision of front setbacks that can accommodate high quality landscape treatments and the display of goods.

To ensure landscape treatments and signage support and enhance the Goulburn Valley Highway as a key gateway to Shepparton, complement the existing Kialla Lakes Drive and Goulburn Valley Highway landscapes, and reinforce landscaped street edges.

To ensure new development provides clearly defined, legible and attractive gateways to residential areas, particularly at Kialla Lakes Drive, and an appropriate built form and landscape interface between commercial and residential areas.

To implement the design and development guidelines for the Kialla Park Boulevard along the Goulburn Valley Highway and Kialla Lakes Drive in accordance with the *Urban Design Framework – Shepparton North & South Business Areas 2006* and the *Addendum to the Urban Design Framework – Shepparton North & South Business Areas 2017*.

2.0
DD/MM/YYYY
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Buildings and works

Permit requirements

A permit is required to:

- Construct a fence that exceeds 1.2m in height above natural ground level, where the fence is located along a boundary that has a street frontage, and has a transparency ratio of less than 50%, including piers, columns and bases.

Design Requirements

The following buildings and works requirements apply to an application to construct a building or to construct or carry out works, to the satisfaction of the responsible authority:

Building height

- Buildings should be a maximum height above natural ground level as specified in the table below.
- The street wall height of a building facade should not exceed the height above natural ground level specified in the table below.

<i>Precinct (shown on Map 1)</i>	<i>Building height</i>	<i>Street wall height</i>
Goulburn Valley Highway Precinct	14.5m maximum	8m or three storeys
Kialla Lakes Drive Precinct	11m maximum*	6.9m or two storeys*

GREATER SHEPPARTON PLANNING SCHEME

Building setbacks

- Building setback should be 20 metres from the front boundary.

~~A lesser~~ The building setback (not less than 9 metres) may be ~~considered by the responsible authority reduced to a minimum of 9 metres from the front boundary~~ for the properties listed below for a ~~single~~ dwelling in a ~~General Residential Zone, Neighbourhood Residential Zone or a Residential Growth Zone.~~

- No. 2 Reserve Street Kialla (Lot No. 1 PS327036F Parish of Kialla)
- No. 4 Reserve Street Kialla (Lot No. 2 PS327036F Parish of Kialla)
- Nos 1/6 and 2/6 Reserve Street Kialla (Lot No. 3 LP63440 Parish of Kialla)
- No. 10 Reserve Street Kialla (Lot Nos. 4 and 5 LP63440 Parish of Kialla)
- No. 12 Reserve Street Kialla (Lot No. 6 LP63440 Parish of Kialla)
- No. 14 Reserve Street Kialla (Lot No. 7 LP63440 Parish of Kialla)
- No. 16 Reserve Street Kialla (Lot No. 8 LP63440 Parish of Kialla)
- No. 1 Raftery Road Kialla (Lot No. 7 LP115007 Parish of Kialla).

A lesser setback (not less than 9 metres) may be considered by the responsible authority for the properties listed below for buildings and works in a Commercial Zone or Industrial Zone.

- Lot 1 and Lot 2 PS433159.
- Lot 1, Lot 2 and Lot 3 PS439192.
- Lot 2 PS426100.
- Lot 1 and Lot 2 LP121036.

Parking and access

- Encourage vehicular access points and driveways to be designed to allow convenient, safe and efficient vehicle movements and connections within the development and to the street network. All vehicles including those delivering to or servicing the site should be able to enter and exit the site in a forward direction.
- A vehicular access point across lot frontage is to be no more than 6.4 metres wide
- Driveways should be designed to minimise any conflict of vehicle movements with pedestrians including consideration of side fences and / or buildings that will impact on sight lines between pedestrians and vehicular traffic.

The following requirements apply to non-residential applications:

- Adequate on site parking in accordance with the Greater Shepparton Planning Scheme should be provided in a form and manner that will not reduce the amenity of the area.
- Encourage developers to consider planting trees throughout car parking areas.

Landscaping

- For interfaces between residential and non-residential properties, ~~A~~ a minimum 3 metre landscape buffer must be provided along rear boundaries to ensure suitable interface to residential properties. Where a residential ~~zone~~ property abuts a side boundary of a non-residential property, a minimum 1.5m landscape buffer must be provided.

GREATER SHEPPARTON PLANNING SCHEME

- **For non-residential applications.** A minimum of 15% of the area to the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree with a mature height of 10 metres (except where the land is also affected by DDO2, when the maximum height should not exceed 7.5metres, except with a Permit issued pursuant to that Schedule). All plant species must be suitable to this area and to the Council's satisfaction.
- The front building setback should be landscaped to include a variety of shrubs and ground covers. Plant species should be suitable to this area, reflect streetscape planting schemes and be to the satisfaction of the responsible authority.
- Paving materials that provide texture, patterns, subtle colour and permeability to the lot frontage should be used. Large expanses of solid paving such as grey cement or asphalt should be avoided.
- Incorporate small to medium sized trees wherever possible to provide scale, aesthetic relief and shade to front entrances.
- Environmental weeds and invasive tree species should be avoided to the satisfaction of the responsible authority.

Application Requirements

An application for buildings and works must be accompanied by the following information, to the satisfaction of the responsible authority, as relevant:

- Applications within the Sub-Regional Centre (Riverside Plaza) should demonstrate how the proposal will support the site's ongoing function as a key component of the Kialla Lakes Boulevard Precinct.
- A landscaping plan together with proposed irrigation system should be submitted with applications for buildings and works to the satisfaction of the responsible authority.
- The landscape plan and irrigation system should implement water wise, water sensitive urban design and low water use plant materials to the satisfaction of the responsible authority.

Exemption from notice and review

An application for construction of a building or to construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

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Subdivision

None specified.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0
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Proposed C196

Advertising signs

Advertising sign requirements are at Clause 52.05. All land located within the Kialla Park Boulevard Precinct is in Category 1 – Commercial Areas.

GREATER SHEPPARTON PLANNING SCHEME

In addition to the relevant requirements at Clause 52.05, an application for advertising signage should meet the following requirements, to the satisfaction of the responsible authority:

- One business identification sign is permitted per development.
- Multiple business occupancies are to share space on the sign.
- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2 metres, and a maximum width of 1.5 metres. This envelope includes the height of any supporting structure.
- Signs attached to a building are encouraged and should be a maximum height of 1.0 metre, and a maximum width of 3.0 metres. Exemptions may be made for signs composed of individual letters that form an integral part of the building façade.
- Panel, Promotion and Major Promotion signs should be avoided.
- Above verandah signs including V-board signs and advertising elements such as banners, flags and inflatable should be avoided.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.
- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are electronic, animated, contain moving parts or have flashing elements are not encouraged.
- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.
- Refer to Clause 52.05 of the Shepparton Planning Scheme for general requirements relating to signage.

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Proposed C196

Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the proposal provides for a vibrant and active commercial development that reinforce the character of the area with consistent front setbacks for the display of goods in a well landscaped boulevard setting.
- Whether the height of a proposed building accords with the scale of the local environment and the type of surrounding buildings.
- The effect of the development of proposed buildings on the amenity of abutting buildings.
- Whether the proposed landscape treatment contributes to the character of Kialla Lakes Drive or the Goulburn Valley Highway landscape gateways.
- Whether the design, siting and appearance of buildings improves visual presentation of the frontages at the town entrance.
- The architectural quality and innovative response of the building design.
- Whether building setbacks provided along Kialla Lakes Drive or Goulburn Valley Highway demonstrate appropriate consideration of the streetscape and the residential interface.
- Whether the layout allows for safe access and egress from the site.
- The location of any proposed car parking.

GREATER SHEPPARTON PLANNING SCHEME

- The inclusion of design elements which protect the amenity of abutting residents.
- Whether the development provides for a robust, suitably scaled landscape treatment that integrates the built form, provides an appropriate scale, reduces its visual impact and provides a suitable interface between the commercial and the residential areas.
- Whether the development provides landscape treatment that visually integrates the commercial areas and contributes to a pleasing streetscape, while also partially screening the buildings when viewed from the road.
- Whether the proposed development contributes to the significance of the Sub-Regional Centre providing specialty services to surrounding residential and business areas.
- Whether the proposed development on land abutting a residential zone complies with the overlooking and overshadowing provisions set out at Clause 54 or Clause 55, as appropriate.
- Whether the design considered energy and resource efficient and sustainable design principles.
- Whether the proposal is in accordance with the design and development guidelines set out in the *Urban Design Framework - Shepparton North & South Business Areas*, July 2006 and the *Addendum to the Urban Design Framework* (Planisphere, 2017).
- Whether the proposal complies with the provisions of DDO2 of this planning scheme to the satisfaction of the Responsible Authority.

GREATER SHEPPARTON PLANNING SCHEME

DIAGRAM 1: BUILDING HEIGHT AND SETBACK IN KIALLA LAKES DRIVE PRECINCT

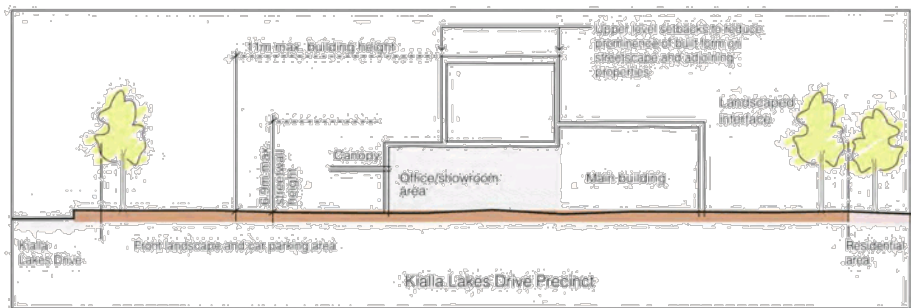
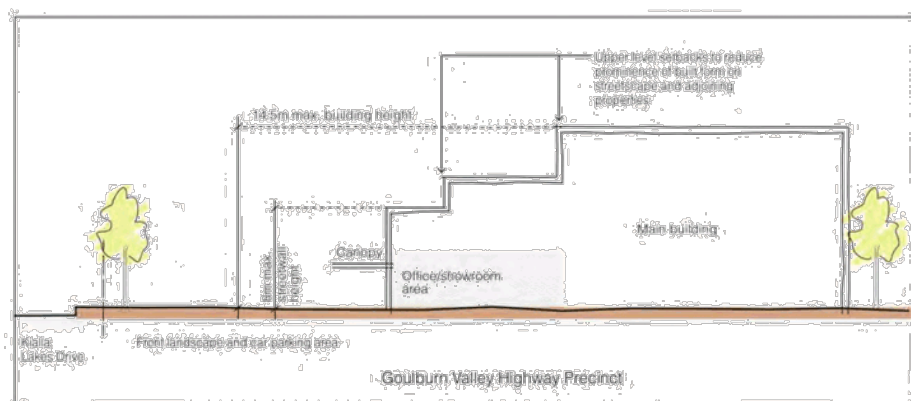


DIAGRAM 2: BUILDING HEIGHT AND SETBACK IN GOULBURN VALLEY HIGHWAY PRECINCT



GREATER SHEPPARTON PLANNING SCHEME

DIAGRAM 3: FREESTANDING BUSINESS IDENTIFICATION SIGN ENVELOPE

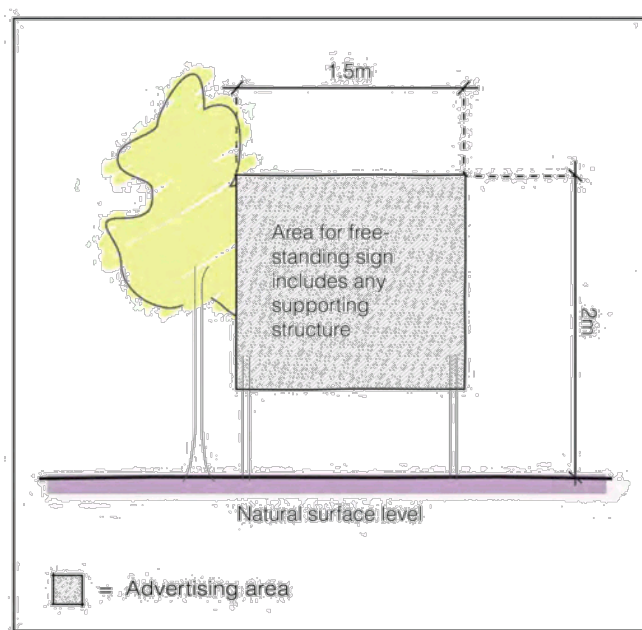
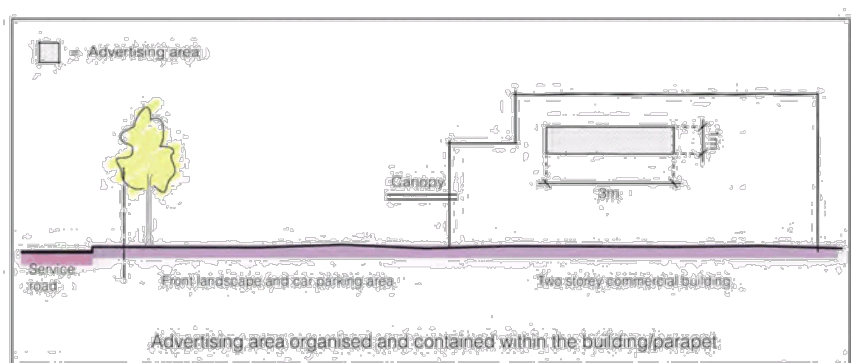
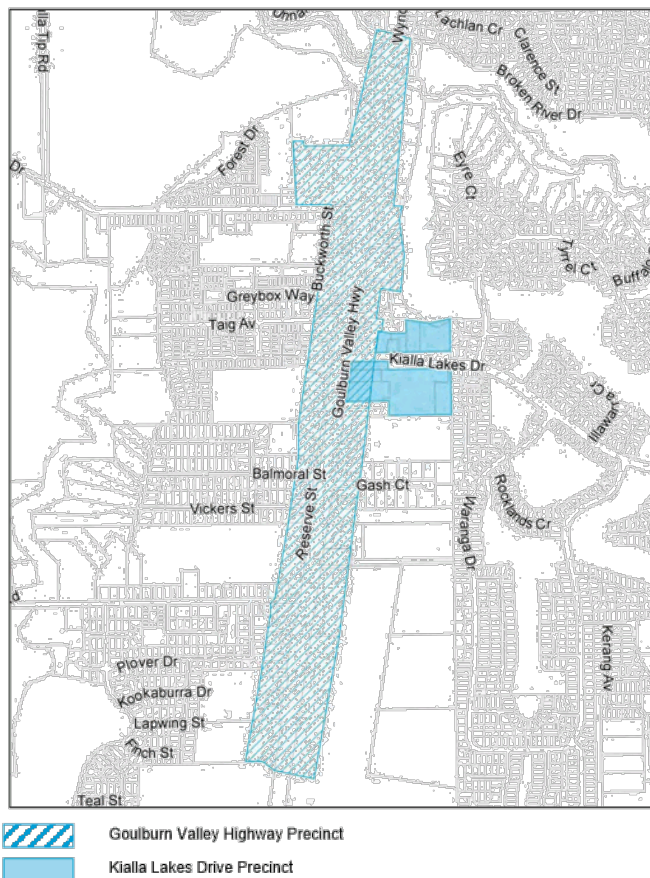


DIAGRAM 4: SIGNS ATTACHED TO BUILDINGS



GREATER SHEPPARTON PLANNING SCHEME

MAP 1: PRECINCTS



GREATER SHEPPARTON PLANNING SCHEME

DD/MM/YYYY
Proposed C196**SCHEDULE 9 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY**Shown on the planning scheme map as **DDO9**.**SHEPPARTON NORTH GATEWAY COMMERCIAL PRECINCT****1.0**DD/MM/YYYY
Proposed C196**Design objectives**

To encourage a vibrant commercial area supporting large scale, regionally significant service and retail activities that reflect the strategic importance of the area as a gateway location in Shepparton's north.

To ensure the design, quality and siting of buildings reflects and enhances the role of the area while making efficient use of the land. This includes at prominent corners and sites at the northern and southern extents of the precinct, where innovative, high quality design is particularly important to strengthen the gateway role of the area.

To ensure signage is appropriately designed, sited and scaled for the gateway corridor context and minimises visual clutter.

To maintain a sense of openness and views to the surrounding landscape corridor from the Goulburn Valley Highway, and integrate public realm planting and a high quality public that appropriately within the surrounding landscape.

To implement the design and development guidelines for the the Goulburn Valley Highway in accordance with the *Urban Design Framework – Shepparton North & South Business Areas 2006* and the *Addendum to the Urban Design Farmework – Shepparton North & South Business Areas 2017*.

2.0DD/MM/YYYY
Proposed C196**Buildings and works**

A permit is required to:

- Construct a fence that exceeds 1.2m in height above natural ground level, where the fence is located along a boundary that has a street frontage and is:
 - Visually impermeable; or
 - Constructed of unpainted or galvanised steel or uncoated wire.

Design Requirements

The following buildings and works requirements apply to an application to construct a building or to construct or carry out works, to the satisfaction of the responsible authority:

Building design

- The size of new buildings should be in keeping with the large format scale of surrounding development.
- Built form should be articulated and maximise glazing to avoid blank and dominant façades and create an appropriate transition between pedestrian entries, offices and warehouse areas.
- Offices or building areas open to the public should be located at the front of the building massing.
- Upper level building setbacks should be used to reduce building bulk and add visual interest.
- Street setbacks must incorporate significant landscaping and sealed car parking, and be utilised to provide clear sightlines and legibility to the street.
- Building roof form should be designed to complement the prevailing character.

GREATER SHEPPARTON PLANNING SCHEME

- Plant equipment, vents and any other mechanical equipment must be carefully designed and sited or incorporated into the roof design, to ensure it is screened or concealed from the street, surrounding public spaces and buildings.
- Where possible, ensure verandahs and canopies are incorporated into single storey built form components to enhance site amenity and provide shade for employees and visitors to the site.
- Sustainable water use in buildings should be provided by implementing measures to collect rain water runoff from roof areas, and ensure water storage/tanks are located away from public view, and do not impact on neighbours' visual amenity.
- Materials for external surfaces of new developments should consider and complement those used in existing buildings in the area, such as brick and non-reflective, high quality cladding.
- The use of concrete wall construction (e.g. tilt panels) should be complemented by the use of contrasting claddings such as timber and metals that provide facade definition.
- Reflective glass, PVC, unrelieved painted render, highly reflective materials such as zincalume, unarticulated concrete surfaces and unarticulated cladding systems should be avoided. Bright, extravagant colours schemes should also be avoided.
- Colour schemes of all external surfaces of new developments must complement those found in the surrounding area and should consider neutral colours.

Building height

- Buildings should be a maximum height of 11m above natural ground level, plus any applicable flood level determined by a relevant referral authority pursuant to this scheme. A building may exceed the maximum height by up to 1 metre if the slope of the natural ground level, measured at any cross section of the site of the building wider than 8 metres, is greater than 2.5 degrees. In that case, the building height should be 10 metres plus any applicable flood level determined by a relevant referral authority pursuant to this scheme. See Diagram 3.
- The street wall height of a building frontage should not exceed two storeys (or 6.9m) above natural ground level. See Diagram 3.

Building setbacks

- Building setback should be a minimum of 9m and maximum of 20m from the front lot boundary, to the satisfaction of the responsible authority. A lesser setback may be considered by the responsible authority for lots that do not abut a Road Zone Category 1. See Diagram 4.
- ~~For interfaces between residential and non-residential properties~~ Where a zero (0m) setback is sought on land adjacent to the GRZ1, the overshadowing and overlooking provisions of ~~Clause 55~~ the Greater Shepparton Planning Scheme must be considered to limit impacts of commercial development on residential amenity with regard to overshadowing and overlooking.
- Where possible, development should be set back from at least one side boundary by a minimum of 1m to enable views from the Goulburn Valley Highway between buildings to the surrounding area.
- For lots that abut a Road Zone Category 1, Lloading and unloading areas should be set back a minimum distance of 20m from the front lot boundary.

Fences

- Fencing should be constructed from materials that complement the building and surrounding area and should be painted in muted colours/tonings, avoiding galvanised steel mesh and 'Colorbond' style fencing.
- Front fences should have a minimum 50% visual permeability.
- Where possible, fencing should be softened and screened by vegetation planting.

GREATER SHEPPARTON PLANNING SCHEME

Parking and access

- Sufficient car parking spaces should be provided on site for occupants and visitors in accordance with Clause 52.06 of the Greater Shepparton Planning Scheme.
- For larger sites, consideration should be given to short-term parking areas located in the front setback, with longer-term parking (i.e. staff parking) located to the rear.
- Avoid large paved car parking areas that are not broken up by landscaping.
- Ensure a landscape buffer is provided between car parking areas and the front property boundary.
- Where practicable, use landscape design to provide significant canopy trees that can assist in shading car parking areas to mitigate heat impacts.
- Where practicable, limit vehicle entry points to one crossover and avoid large areas of circular driveway aprons and impermeable surfaces.
- Group access points and limit double crossover widths to large sites to minimise opportunities for vehicle conflict between ~~the~~ service roads and [a Road Zone Category 1 GVH](#).
- For corner sites, encourage vehicle access to a side road rather than the [Goulburn Valley Highway](#) service roads.
- For the purpose of providing an active street frontage, not more than 7.5m on any site frontage should be utilised for vehicular access purposes. See Diagram 6.

Landscaping

- ~~A minimum 1.5m landscape buffer must be established along rear boundaries to ensure suitable interface with adjoining residential lots in the GRZ1. See Diagram 5.~~
- [For interfaces between residential and non-residential properties](#), ~~A~~ minimum 1.5m landscape buffer should be established to ~~ensure suitable interface with all other adjoining zones~~ [limit impacts of commercial development on residential amenity](#). See Diagram 5
- A minimum of 15% of the lot frontage must be landscaped to include a variety of shrubs and at least one significant tree (mature height 10m). Plant species must be appropriate and suitable to this area, and to Council's satisfaction.
- Provide large canopy trees and understorey planting to minimise loss of views from the [Goulburn Valley Highway](#) corridor to the surrounding landscape.
- Minimise areas of paving, particularly in the front setback.

Advertising signage

- Panel, Promotion and Major promotion signs will only be considered acceptable on a temporary basis (e.g. a maximum of 15 years from the date of permit issue).
- Free-standing signage (e.g. pole signs) must be set back a minimum distance of 1m from the front property boundary.
- Signage, including painted signs, located on a side wall should not occupy more than 50% of the area of the wall.
- Freestanding business identification signs are to fit in an envelope that is a maximum height of 2m and maximum width of 1.5m. This envelope includes the height of any supporting structure. See Diagram 1.
- Signs attached to a building are encouraged and should be a maximum height of 1m and a maximum width of 3m. Exemptions may be made for signs composed of individual letters that form an integral part of the building facade. See Diagram 2.

Application Requirements

An application for buildings and works must be accompanied by the following information, to the satisfaction of the responsible authority, as relevant:

GREATER SHEPPARTON PLANNING SCHEME

- A written response detailing how the proposal appropriately responds to the objectives and provisions of the Schedule.
- A detailed landscape plan and irrigation system demonstrating how the development will implement water wise, water sensitive urban design and low water use plan materials, to the satisfaction of the responsible authority.

An application to vary the requirements set out in a Sub-clause to this schedule must provide a written submission demonstrating how the proposal responds to the strategic ~~importance and gateway significance~~ of the area as a gateway corridor, while providing:

- Innovative, high quality design;
- Efficient use of the land; and
- Enhancement of the appearance and strengthening the identity of the commercial corridor.

This requirement applies to proposals on prominent sites such as corners and sites at the northern and southern extents of the precinct.

Exemption from notice and review

An application for construction of a building or to construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

3.0

DD/MM/YYYY
Proposed C196

Subdivision

None specified.

Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0

DD/MM/YYYY
Proposed C196

Advertising signs

Advertising sign requirements are at Clause 52.05. All land located within the Shepparton North Gateway Commercial Area Precinct is in Category 1 – Commercial Areas.

In addition to the relevant requirements at Clause 52.05, an application for advertising signage should meet the following requirements, to the satisfaction of the responsible authority:

- Business identification signage should form an integral part of the front building façade, be appropriately proportioned to sit within the parapet or gable end, and not protrude above or beyond the building façade.
- Business identification information can be incorporated into one way-finding oriented sign at the entrance to each section of service road to reduce visual clutter.
- Major promotion signs should be attached to and integrated into the front building facade.
- High wall and sky signs are discouraged.
- Panel and Promotion signs that project outside the perimeter of the building, or are free-standing, are discouraged.
- Internally illuminated signs should be located below the main roofline and integrated into the building form/facade.

GREATER SHEPPARTON PLANNING SCHEME

- Where floodlit signs are proposed, consideration should be given to the impact of glare, light spill and shadow cast.
- Signs that are electronic, animated, contain moving parts or have flashing elements are not encouraged.
- Bunting, animated and reflective signs, including sandwich boards and inflatable and temporary signs, are discouraged.
- Colours, materials, animation and illumination that interfere with the safety or efficiency of traffic circulation, safety or function of the GVH as a major thoroughfare and Road Zone, Category 1 (RDZ1) must be avoided.
- The design of new buildings should take into account the likely need for signage by future occupants, and incorporate sign panels that meet these guidelines.

5.0DD/MM/YYYY
Proposed C186**Decision guidelines**

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The design and development guidelines set out in the *Addendum to the Urban Design Framework - Shepparton North & South Business Areas* (Planisphere, 2017).

Building design and siting

- Whether the height of a proposed building accords with the scale of the local environment and type of surrounding buildings.
- The design and siting of existing development in the area.
- The interface with development on adjoining zones outside the Commercial 1 Zone (~~C1Z~~) or Commercial 2 Zone (~~C2Z~~).
- Whether the proposed development appropriately responds to and enhances the character of the *Shepparton North Gateway Commercial Precinct* and its gateways.
- Whether the proposed front setbacks enable efficient use of the land.
- Whether opportunities for views between buildings to the surrounding area are available.
- Whether there is any potential for land use conflicts with adjoining zones as a result of the proposed building siting.
- Whether the development has been designed in a manner that is climatically appropriate for the local context.
- The potential for the proposed building to impact visual amenity of surrounding lots.
- Whether the proposed building incorporates active and attractive elements that interact with the streetscape.
- Whether, in seeking to vary any requirements in this Schedule, the proposed development:
 - Achieves innovative, high quality design and efficient use of the land; and
 - Enhances the appearance and strengthens the identity of the commercial corridor.

Landscaping

- Whether adequate space has been provided for landscaping within the front setback.
- Whether plant choice is sustainable and suitable within the local environment.

GREATER SHEPPARTON PLANNING SCHEME

- Where front fencing is proposed, whether the proposed fence enhances the street appearance and maintains view to, between, and around sites.
- Whether the proposed landscape concept plan responds to Council's *Landscape Plan Guide* (Spiire, 2017) and any relevant planting schedule.

Advertising signage

- Whether the proposal is appropriately designed, sited and scaled for the Goulburn Valley Highway corridor without detracting from the public realm or competing with way-finding and road signage.
- Whether colours and materials proposed might interfere with the safety or efficiency of the Goulburn Valley Highway as a Road Zone, Category 1 (RDZ1).

GREATER SHEPPARTON PLANNING SCHEME

Diagram 1: Freestanding business identification sign envelope

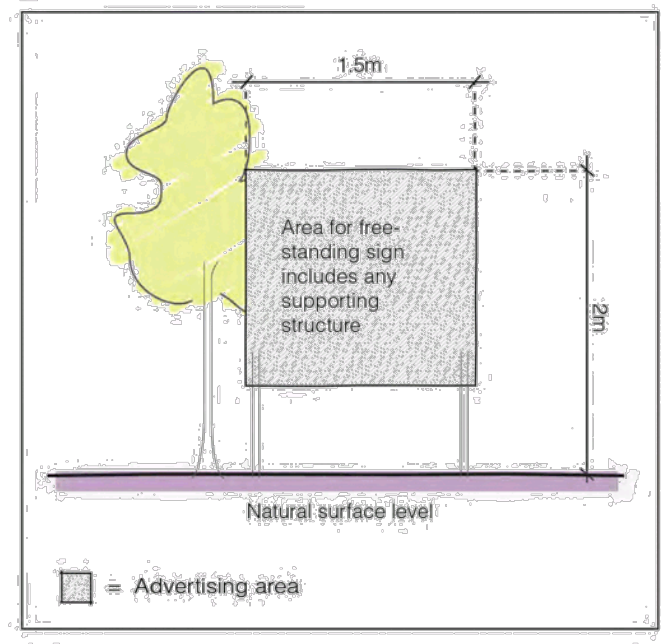


Diagram 2: Signs attached to buildings

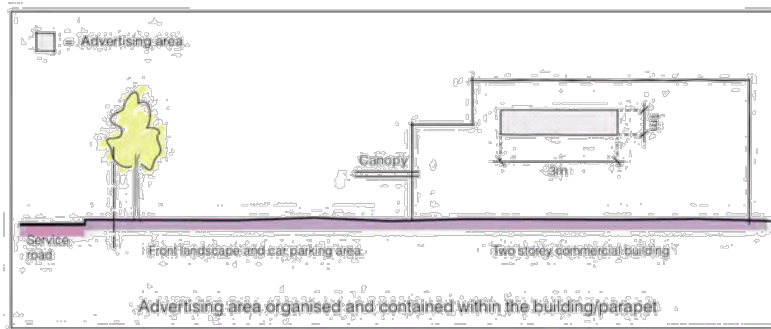
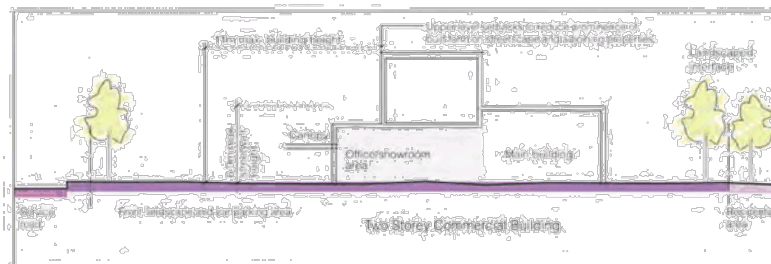


Diagram 3: Building heights and setbacks



GREATER SHEPPARTON PLANNING SCHEME

Diagram 4: Site layout and design

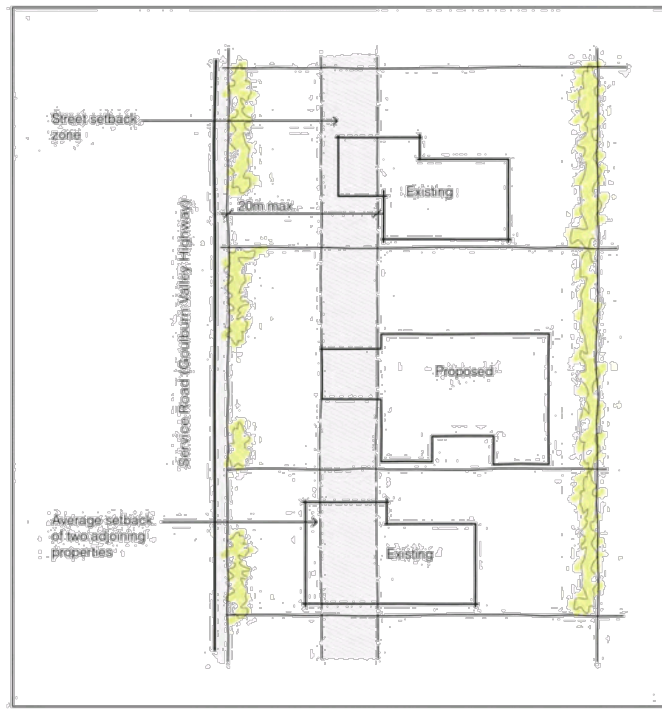
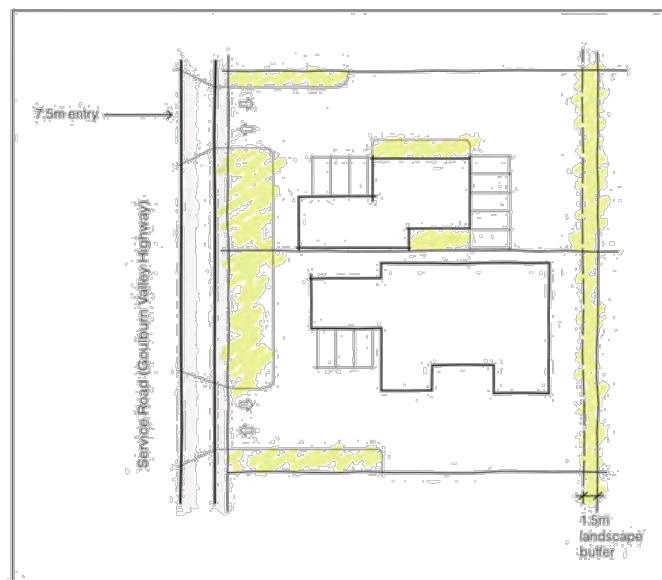
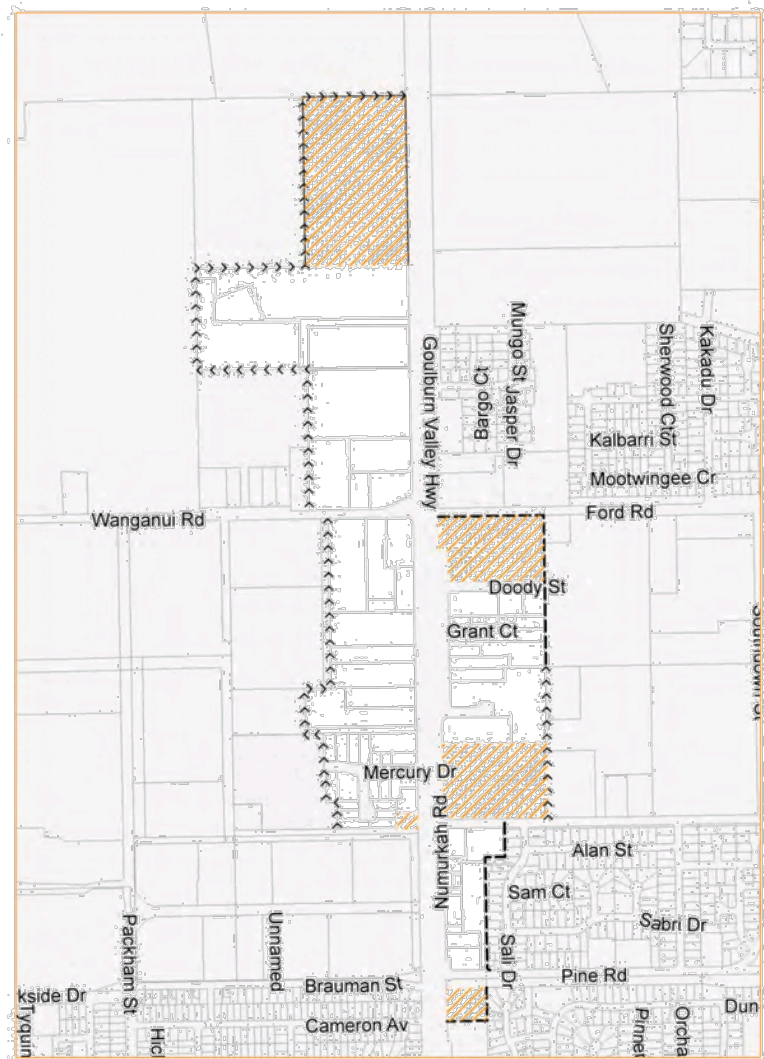


Diagram 5: Landscape buffer








GREATER SHEPPARTON PLANNING SCHEME

Map 1: Framework Plan



Legend

-  Land outside subject area
-  Prominent sites (including gateways)
-  Property boundaries
-  Interface with adjoining residential-zoned land
-  Interface with other non-commercially-zoned land



GREATER SHEPPARTON PLANNING SCHEME

03/10/2017
GC13
Proposed C196

SCHEDULE TO CLAUSE 61.03 WHAT DOES THIS SCHEME CONSIST OF?

1.0 Maps comprising part of this scheme:

03/10/2017
GC13
Proposed C196

Zoning and Overlay maps

- 1, 1HO, 1LSIO-FO, 1BMO
- 2, 2DPO, 2ESO, 2HO, 2LSIO-FO, 2PAO, 2BMO
- 3, 3HO, 3LSIO-FO, 3PAO
- 4, 4HO, 4LSIO-FO
- 5, 5HO, 5LSIO-FO
- 6, 6EAO, 6DPO, 6ESO, 6HO, 6LSIO-FO
- 7, 7DPO, 7HO, 7LSIO-FO
- 8, 8DPO, 8HO, 8LSIO-FO
- 9, 9DPO, 9ESO, 9HO, 9LSIO-FO
- 10, 10DPO, 10ESO, 10HO, 10LSIO-FO
- 11, 11AEO, 11DCPO, 11DDO, 11DPO, 11EAO, 11ESO, 11HO, 11LSIO-FO, 11PAO, 11VPO, 11BMO
- 12, 12DPO, 12HO, 12LSIO-FO, 12PAO, 12BMO
- 13, [13DDO](#), 13DPO, 13EAO, 13 HO, 13LSIO-FO, 13PAO
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- 18, 18DDO, 18HO, 18LSIO-FO, 18PAO, 18PO
- 19, 19DDO, 19DPO, 19LSIO-FO, 19PAO
- 20, 20DCPO, 20DDO, 20DPO, 20HO, 20LSIO-FO, 20PAO, 20BMO
- 21, 21HO, 21LSIO-FO, 21BMO
- 22, 22AEO, 22DDO, 22DPO, 22HO, 22LSIO-FO, 22PAO, 22PO, 22RXO, 22BMO
- 23, 23DPO, 23EAO, 23HO, 23LSIO-FO, 23PAO, 23PO, 23BMO
- 24, 24DDO, 24DPO, 24LSIO-FO, 24PAO
- 25, 25DPO, 25LSIO-FO, 25PAO, 25BMO
- 26, 26AEO, 26DCPO, 26DDO, 26DPO, 26EAO, 26LSIO-FO, 26BMO
- 27, 27DPO, 27LSIO-FO, 27BMO
- 28, 28EMO, 28HO, 28LSIO-FO, 28SMO, 28BMO
- 29, 29EMO, 29HO, 29LSIO-FO, 29SMO, 29BMO
- 30, 30LSIO-FO
- 31, 31ESO, 31HO, 31LSIO-FO, 31PAO, 31SMO, 31BMO
- 32, 32ESO, 32HO, 32LSIO-FO, 32SMO, 32BMO
- 33, 33HO, 33LSIO-FO, 33PAO, 33BMO
- 34, 34HO, 34LSIO-FO, 34PAO, 34SMO, 34VPO, 34BMO
- 35, 35LSIO-FO, 35SMO
- 36, 36LSIO-FO, 36SMO, 36BMO

GREATER SHEPPARTON PLANNING SCHEME

- 37, 37HO, 37LSIO-FO, 37SMO, 37BMO

*Planning and Environment Act 1987***GREATER SHEPPARTON PLANNING SCHEME****AMENDMENT C196****INSTRUCTION SHEET**

The planning authority for this amendment is the Greater Shepparton City Council.

The Greater Shepparton Planning Scheme is amended as follows:

Planning Scheme Maps

The Planning Scheme Maps are amended by a total of two attached map sheets.

Overlay Maps

1. Amend Planning Scheme Map No. 26DDO in the manner shown on the one attached map sheet marked "Greater Shepparton Planning Scheme, Amendment C196".
2. Insert new Planning Scheme Map Nos. 13DDO and 14DDO in the manner shown on the one attached map sheet marked "Greater Shepparton Planning Scheme, Amendment C196".

Planning Scheme Ordinance

The Planning Scheme Ordinance is amended as follows:

3. In Local Planning Policy Framework – replace Clause 21.09 with a new Clause 21.09 in the form of the attached document.
4. In Overlays – Clause 43.02, replace Schedule 7 with a new Schedule 7 in the form of the attached document.
5. In Overlays – Clause 43.02, insert a new Schedule 9 in the form of the attached document.
6. In General Provisions – Clause 61.03, replace the schedule with a new Schedule in the form of the attached document.

End of document